

Edmundson Plaza

Docket Item #14 A-C

Development Special
Use Permit #2006-0023 (A)

Transportation Management
Plan #2007-0018 (B)

Vacation #2007-0001 (C)

1708 & 1710 Prince Street,
1703, 1705, 1707 Duke Street &
206 Reinekers Lane

Planning Commission Meeting
June 5, 2007



Docket Item #14
DSUP #2006-0023
SUP TMP #2007-0018
VAC#2007-0001
EDMUNDSON PLAZA

Planning Commission Meeting
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REQUEST: Consideration of a request for approval of: 1) a development special use permit, with site plan for an office building with ground-floor retail and underground parking; 2) a special use permit for a Transportation Management Plan; 3) a special use permit for an increase in the penthouse height; 4) a special use permit for increased floor area; 5) a special use permit for a health club; 6) a special use permit for a restaurant use; and 7) a vacation of a portion of the public right-of-way on Reineker's Lane.

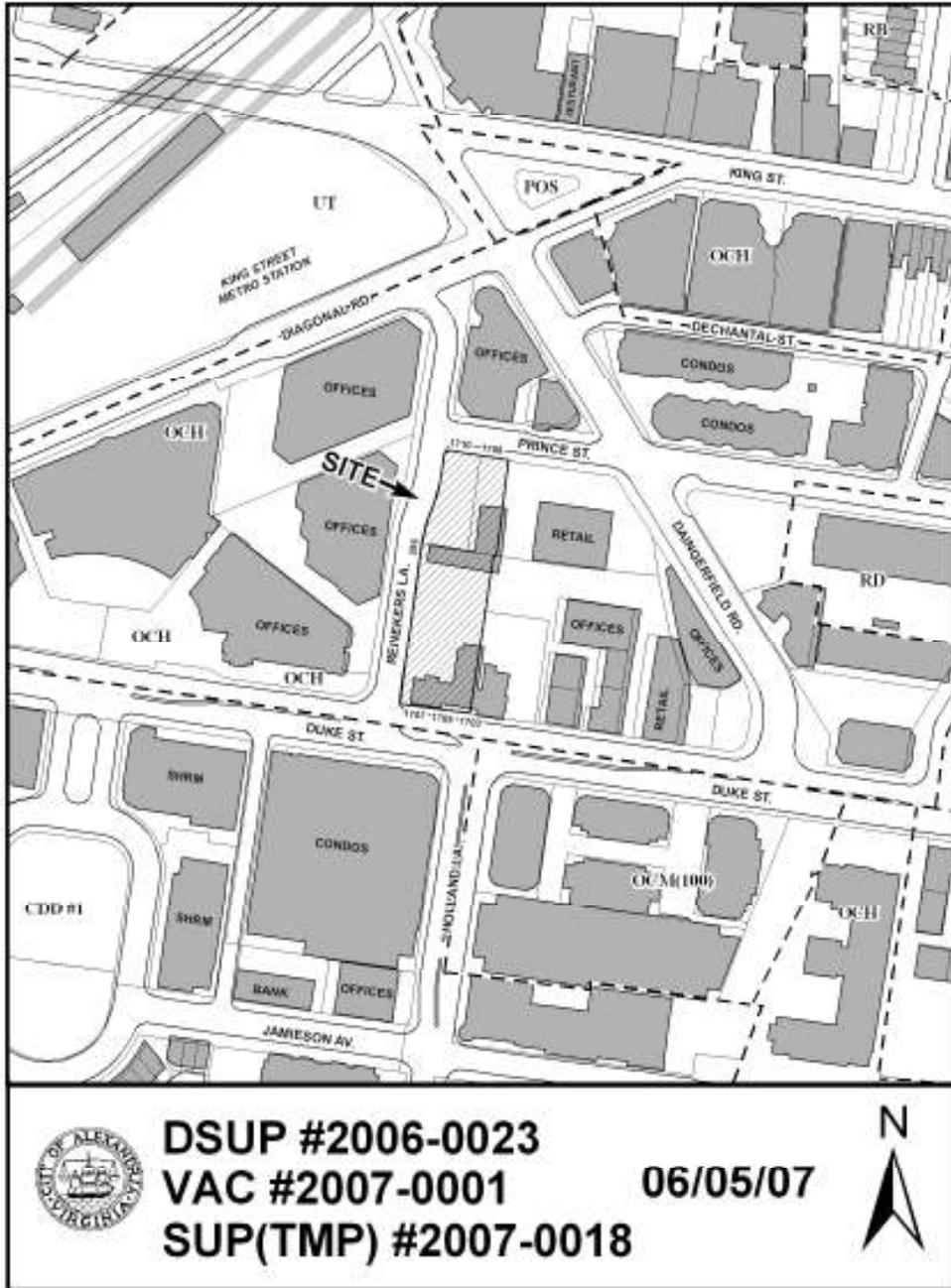
APPLICANT: Carr Properties, Inc, by Jonathan Rak, attorney

STAFF: Jeffrey Farner, Division Chief, Development
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LOCATION: 1708 Prince Street, 1710 Prince Street, 1703, 1705, 1707 Duke Street & 206 Reinekers Lane

ZONE: OCH/Office Commercial High

DSUP #2006-0023, TMP#2007-0018, VAC#2007-0001
Edmundson Plaza



I. IMPACTS / BENEFITS:

TABLE #1

IMPACT/BENEFITS	COMMENTS
Consistency with Strategic Plan	<ul style="list-style-type: none"> The proposal will add an active Metro oriented development to the King Street Metro urban setting, with a five-story office building and ground floor retail uses, and a publicly accessible pedestrian open space plaza which will serve as a setting for the historic, 100-year old building being retained.
Use	<ul style="list-style-type: none"> 100,108 sq. ft office 15,800 sq. ft. retail 12,000 sq. ft. (below grade) retail-health club
Open Space	<ul style="list-style-type: none"> 8,238 sq. ft of street-level, publicly accessible open space Open space plaza provides a setting for the 100-year old building and a memorial to African American history of the site.
Pedestrian	<ul style="list-style-type: none"> Open space pedestrian plaza connects to other pedestrian pathways and to the King Street Metro. 14-16 ft wide sidewalk along Reineker’s Lane and Prince Street, improved sidewalk width along Duke Street, street trees, pedestrian scale lighting, trash receptacles, and bicycle parking. Ground floor retail accessible to pedestrians along Reineker’s Lane.
Building Compatibility	<ul style="list-style-type: none"> 5-story building with a height of approximately 68 feet, consistent with surrounding area and King Street Metro area. Significant setback around historic building to emphasize and not detract from it.
Affordable Housing	<ul style="list-style-type: none"> \$191,862 contribution to the Housing Trust Fund, consistent with the DHCPWG recommendations.
Parking	<ul style="list-style-type: none"> Three-levels of underground parking with 179 parking spaces. 30 parking spaces proposed for the health club; 6 parking spaces proposed for the retail; and 143 parking spaces proposed for the office use.
Environment	<ul style="list-style-type: none"> Staff has included a recommendation that will require the site and building elements to be designed in a manner to achieve a minimum of 23 points on the LEED rating system
Fiscal	<ul style="list-style-type: none"> \$312,109 Estimated annual real estate tax revenue for the City.

II. EXECUTIVE SUMMARY:

A. Overview:

The applicant is requesting development special use permit approval to construct a five-story office building to consist of approximately 16,000 sq. ft. of retail on the ground floor and 100,000 sq. ft. of office use on the upper four floors. The proposal also includes a 12,000 sq. ft. below-grade retail-health club and three levels of underground parking with 179 parking spaces.



Figure 1: View from Duke St.-Reineker's Lane

Two existing small buildings constructed in the 1950s will be demolished to make room for the proposed building. However, a critical component of the development will retain the 100-year old, small-scale building at the corner of Duke Street and Reineker's Lane. As a result, the proposal combines the City's interest in historic preservation and increased density near the King Street Metro.



Figure 2: Site Plan in Aerial

The approvals requested by the applicant consist of the following:

- Increased floor area from 2.0 to 3.0 FAR;
- Transportation Management Plan (TMP);
- Penthouse height to exceed 15 feet (18 ft. proposed);
- Reduced office parking ratio;
- Vacation of a portion of the right of way;
- Full-service restaurant use; and
- Health and athletic club use.



Figure 3: Potential Redevelopment Sites

This site is one of the few remaining undeveloped sites in proximity (approximately 1,000 ft.) to the King Street Metro that has been planned for high density and height. Others include the adjoining Table Talk and Crate and Barrel sites, and the site at the southeast corner of Daingerfield and Prince Street (see Figure 3). The subject property is unique from the others, however, because it includes a small scale, 100-year old building on Duke Street. Therefore, it presents both the challenge and opportunity of retaining a historic building while meeting the planning objectives of appropriate density near the King Street Metro Station. Although the historic building is not within the boundaries of a historic district, it is on the 100-year old building survey and, therefore, any changes or alterations require approval by the Old and Historic Board of Architectural Review.



Figure 4: 100-Year Old Building at 1707 Duke St

To address these two planning interests – historic preservation and increased density – staff worked with the applicant to establish the following principles:

- Provide a considerable (59 to 82 ft) setback between the two buildings;
- Step down the building height in the direction of the historic building;
- Provide ground level open space and landscaping around the historic building;
- Use appropriate wall treatment adjacent to the historic building; and
- Use modulation, materials and colors to reduce the perceived scale of the proposed building.

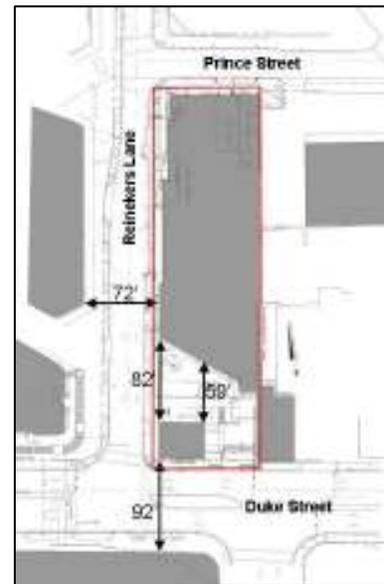


Figure 5: Building Separation

B. Building Compatibility:

The proposed building is considerably larger in height and footprint than the existing historic building. Staff required a series of design features as part of the development to ensure that the two very different buildings remain differentiated but compatible. For example, the proposed building is set back approximately 60 to 85 feet from the existing building



Figure 6: Pedestrian Connection to Metro

(see Figure 5). The distance is significant, and is comparable to the 66-foot width of a typical Old Town street (building-

to-building). The setback provides a transition between the existing building and the new building and also allows space for a prominent ground level open space courtyard. The new building will include a variety of heights, and steps down to 50 feet adjacent to the 100-year old building, providing additional transition between the two. Finally, the proposal includes a pedestrian path through the courtyard to separate the two buildings and serve the dual purpose of also connecting to and reinforcing the adjoining King Street Station complex pedestrian path to the Metro Station (See Figure 6).

C. Historic Recognition/Ground Level Open Space:

The proposed setback between the two buildings allows for a 7,619 sq. ft. open space area to be visible from Duke Street and accessible for pedestrians. Because the success of the open space and its opportunity to memorialize the history of the site will only be achieved through appropriate treatment and design, staff has added a series of conditions on this point. In addition, staff recommends that after staff, the applicant and other relevant groups have worked through design concepts for the open space area, the final design be approved by the BAR.

D. Design Transition for 100-Year Old Building:

Given the juxtaposition of the two buildings, the new building must, in terms of materials and design, provide an appropriate background and setting for its historic neighbor. The proposal consists of a transparent glass wall next to the 100-year old building, a generally acceptable design approach because it provides a lighter wall next to the historic, brick building. Nevertheless, staff has been concerned about the quality and design of the wall because, while there are successful precedents, there are also examples that have been less well executed. In response to staff's concerns, the applicant has indicated that it will use Shakespeare Theatre, located at 6th and F Streets in Washington, D.C., as a reference. The street facade of that building consists of two panels of transparent glazing, the outer layer of which is a clear, thin glass veil that appears to be floating. Staff has added a recommendation that will ensure that the color and installation of the Edmundson Plaza glass wall are of exemplary quality, crucial to the success of this design approach.



Figure 7: Building Backdrop



Figure 8: Shakespeare Theatre

E. Uses:

The applicant proposes to include a 12,000 sq. ft. health club use as part of the new building. The use requires SUP approval. The applicant has also indicated a desire to increase the size of the health club to include floor area on the first floor, although most of the use will be located on the first below grade level. Staff recommends that the health club use not be approved at this time. Rather, it should be considered for a separate SUP approval at a later date, when an operator has been chosen and more information is available regarding the size of the use, the specific applicant/operator, number of employees, and appropriate parking for the use.

F. Community:

Staff and the applicant conducted ten community meetings for this project with the Upper King Street Civic Association, Old Town Civic Association, Old Town Business and Professional Association, the Royalton Condominium Association, the Metropolitan Condominium Association, and the Alexandria Black History Museum. The development process has included extensive community feedback. There are numerous changes and revisions to the proposal that have improved the plan through active citizen involvement.

G. Staff Recommendation:

Staff recommends approval of the proposed commercial building since it meets the goals and objectives outlined in the King Street Metro Small Area Plan, including creating a high-quality office and retail/restaurant development supported by publicly accessible open space. The proposal also historic building and provides an appropriate balance and transition between the existing 100-year old structure and the new, Metro-oriented development.



Figure 9: Reineker's Lane-west side

III. BACKGROUND:

A. **Site and Context:**

The 36,691 sq. ft. (0.84 acre) site is located at the visually prominent intersection of Duke Street and Reineker's Lane, within walking distance (approximately 1,000 ft.) of the King Street Metro station. The site is long and narrow, with three frontages: on Duke Street, Reineker's Lane and Prince Street. The Whole Foods mixed-use building is located across Duke Street directly to the south of this property. Immediately to the west, across Reineker's Lane, are two seven-story office buildings. A six-story office building is located directly to the north on Prince Street. The site is also adjacent to two future redevelopment sites, including the Crate and Barrel Outlet store (immediately adjacent to the east on Prince Street), and the Table Talk restaurant site (adjacent on Duke Street).

This commercial infill location now consists of three lots. On the corner of Duke Street and Reineker's Lane sits a 100-year old, two-story, historic structure (1707 Duke Street), now used as a real estate office, with a second non-historic building, now used for a travel agency, attached. On the two lots fronting Prince Street sit two, attached, non-historic, one-story buildings with associated surface parking lots. The three non-historic buildings were constructed in the 1950's and are proposed to be demolished with this development. The proposed plan would consolidate the three lots into one lot.

The subject property slopes down from Reineker's Lane approximately six feet along Prince Street, and approximately three feet along the front of the property at Duke Street. The size, shape and topography of the site create challenges for development, especially relating to building configuration and size, vehicular access, and visibility of the proposed uses.



Figure 10: Prince St-north side



Figure 11: Whole Foods



Figure 12: Crate and Barrel

B. Proposal:

The proposed office building will face both Duke Street and Reineker's Lane, and will be constructed over a three-level underground parking garage accessed from Prince Street on the northern portion of the site. The primary office entrance for pedestrians to the site will be through a prominent open space area on Duke Street, and the main, lobby entrance to the office building will be through a door facing the proposed open space-courtyard. Pedestrian access to the open space and the retail shops will also be provided from Reineker's Lane.



Figure 13: Reineker's Lane and Prince St. Perspective

The proposed five-story, 115,908 sq. ft. building will be long and narrow, with a building footprint of approximately 270 ft. x 95 ft. The building faces are articulated into a series of vertical bays, with portions of the buildings setback to provide visual interest.

The proposed development includes 100,108 sq. ft. of office on four floors of the new building. The first floor will include 15,800 sq. ft. of retail and restaurant use. A 12,000 square foot health and athletic club is proposed on the first below grade level of space. The main retail/restaurant entrances are proposed through doors facing Reineker's Lane, Prince Street and the open space/courtyard area.

C. Historic Context:

This proposal is adjacent to an existing 100-year old building at 1707 Duke Street that, although outside the boundaries of the City's historic districts, is protected by City Council as a designated 100-year old building of historical or archeological interest.

According to Ethelyn Cox's *Historic Alexandria Street by Street: a Survey of Existing Early Buildings*, 1707 Duke Street "may date from the ownership of John Longden, whose death at his residence in West End was announced April 1, 1830. In March 1844, Longden heirs conveyed it for \$1,500 to Joseph Bruin. An 1853 insurance



Figure 14: 100-Year Old House 1707 Duke St.

policy shows Bruin’s two story brick “Negro jail”, 42 x 32 feet, on this site. In July 1864, it was confiscated by the United States Marshall under the Act of Congress authorizing seize of ‘the property of Rebels’”.

The building is a freestanding early 19th-century, two-and-one-half story, five-bay house, with a gable roof, pedimented dormers, and an entry with pilaster and pediment ensemble. The house has a central hall plan and is a rare surviving example in this area of the City. The building was documented and listed in 1978 for inclusion on the City’s 100-Year Old Building List. Since the 1970s, the building has been used as commercial offices.

The plan proposes to remove the three brick planters that have been added along the sidewalk along the front of the historic structure. They are not believed to be part of the original construction of this building and are not of historic significance. The outside stairways along Duke Street will remain and/or be restored. These changes, as well as the impact of the open space/courtyard, will be considered by the BAR.

IV. MASTER PLAN AND ZONING:

This site falls within the King Street Metro Station/Eisenhower Avenue Small Area Plan, which designates the property for OCH/Office Commercial High zoning, and encourages high-density, mixed-use Metro-oriented development. The OCH zone permits business and professional office uses, retail uses, and it permits health club and restaurant uses with a Special Use Permit. The proposed development complies with the OCH Zone as follows:

TABLE #2

Edmundson Plaza Zoning		
Property Address: 1708 Prince Street, 1710 Prince Street, and 1707 Duke Street		
Total Site Area: 36,691 sq. ft (0.84 acres)		
Zone: OCH zoning district		
Current Use: Parking Lots/vacant buildings, and real estate and travel agency uses.		
Proposed Uses: Office, Retail, Restaurant and Health Club		
	Permitted/Required	Proposed
Uses	Office Retail/restaurant Health Club	100,108 sq. ft office (new building) 2,745 sq. ft office (historic building) 15,800 sq. ft. retail, including one full-service restaurant use* 12,000 sq. ft. health club*
FAR	3.0 Max. with SUP (0.32 existing)	3.0 counting above-grade sq. ft. *

Floor Space	110,073 sq. ft. permitted with 3.0 FAR	110,073 sq. ft. proposed with 3.0 FAR*
Yards/ Setbacks	Building height may not exceed twice the setback from the face of the building to the centerline of the street facing the building.	Setback modification required for building height along Prince Street and a portion of Reineker's Lane
Open Space	None Required	8,238 sq. ft at ground level, including the 7,610 sq. ft. courtyard
Building Height	82 ft. max. with ground floor retail	66 to 72 ft.
Penthouse Height	Penthouse height limited to 15 ft.	Proposed penthouse is 18 feet.*
Parking	142 spaces required for Office 6 spaces required for Retail 30 spaces required for Health Club 178 Total Spaces Required	143 spaces proposed for Office* 6 spaces proposed for Retail 30 spaces proposed for Health Club 179 Total Spaces Proposed On-Site
Loading	1 space required	1 space proposed
* SUP required		

V. STAFF ANALYSIS:

The property is a commercial site within walking distance of the King Street Metro. It currently contains small scale circa 1970s buildings and surface parking lots. It also contains a historic building, and is the location of historic events of a sober character. The property is awkwardly shaped and adjoins additional future redevelopment sites to the east. These circumstances create severe challenges, but opportunities as well.

In its review of this proposal, staff focused on a series of key planning factors, including the critical location of the site for pedestrian connections, the important history of the site and the existing building, the height and scale of existing and future buildings in the vicinity, parking and traffic issues and the opportunity for green building technology. The applicant has worked with staff to reach a complex solution: an urban infill development which addresses the street with ground floor retail uses, providing publicly accessible plaza open space, wider sidewalks and the elimination of surface parking. Through the design of the building, increased setbacks and the treatment of the open space courtyard, the proposal meets the difficult challenge of including an historic building and a large new one on the same site and making them compatible. Finally, the mass and scale of the proposal has been refined to relate to the existing scale and character of the development surrounding the new development.

As with each case, a balancing of planning goals and interests – including underground parking, pedestrian improvements and locating density near the metro stations – has been necessary to achieve an appropriate solution. Significantly, the applicant has revised its plan by providing open space on the corner and providing setbacks at the upper level of the building. Staff's recommended conditions add refinements, such as additional pedestrian improvements, landscaping, refinements to the design of the building, and incentives to encourage the use of mass transit. With these changes and recommendations, staff recommends approval of the proposal, although staff is recommending approval of a subsequent special use permit for the proposed health club.

A. Pedestrian Connections:

One of the most important factors in this development case is its location at a key crossroads to a series of pedestrian destinations, and the high foot traffic already experienced at the site today. The Metro station is only 1,000 feet away. The popular Whole Foods grocery store is directly across the street. Residential development located above Whole Foods and along Holland Lane are within walking distance. Carlyle and PTO are just a few blocks away across Duke Street. On a larger scale, the site is within an easy walk to the King Street retail corridor and Old Town Alexandria. This optimum location makes it imperative that any development at Duke and Reineker's Lane establish strong pedestrian connections to these places as well as to future redevelopment sites.

The development itself takes advantage of existing and future pedestrian traffic by incorporating a large open space area with access to both Duke Street and Reineker's Lane, and by active ground floor uses such as retail and restaurant uses. The open space area provides a pedestrian path that lines up with the existing pedestrian connection through the King Street Station complex. Thus, pedestrians can walk from a point to the east on Duke Street, through the subject development, through the King Street Station buildings, and directly to the King Street Metro Station.



Figure 15: Pedestrian Connections

To further enhance the pedestrian environment, the proposal includes a series of important improvements for the Duke Street frontage including widened sidewalks. Staff's recommendations for Duke Street include the following:

- Relocation of the cobra head light to the central median to maintain a continual 14 to 17 ft. wide sidewalk and reduce the visual obstructions near the 100-year old building and open space;
- Designated and colored crosswalk and count down signal at the intersection of Duke Street and Reineker's Lane;
- Elimination of the raised planters in front of the 100-year old building to increase the sidewalk width, which will require subsequent review by the Board of Architectural Review.
- Scaling back the proposed stairs and walls on Duke Street to allow visibility of the historic structure along Duke Street and to allow a visible and physical connection between this site and future redevelopment of the Table Talk site.
- Additional landscaping within the median.
- Pedestrian count-down signals and enhanced crosswalk to Whole Foods.

In addition to the pedestrian improvements on Duke Street, staff recommends the following for the remainder of the site:

- 14 to 16 ft. wide brick sidewalks on Reineker's Lane and Prince Street;
- Stamped and colored asphalt crosswalks at the intersections of Duke Street and Reineker's Lane, and Prince Street and Reineker's Lane;
- Pedestrian scale lighting;
- Decorative benches and trash receptacles;

- Bicycle parking for retail patrons and office workers;
- Irrigation for the open space;
- Decorative paving for the open space courtyard; and
- Continual row of street trees on the perimeter of the site.

These improvements will complement the strong pedestrian orientation of the proposed development.

B. Open Space:

The proposed open space courtyard on Duke Street provides a pedestrian connection through the site and an opportunity for ground level trees, landscaping and passive space for this urban setting. In this case, the open space does even more as well: it provides support and emphasis for the 100 year-old building and the important history of the site.

The proposed ground-level open space is accessible and visible from the adjacent streets and consists of a courtyard, outdoor seating, plantings, and public art. The space will be visible from Reineker's Lane and the pedestrian alley through the adjacent King Street Station complex. It will also have a prominent entrance on Duke Street, and be visible from a distance on Duke Street as well as from Holland Lane. With the open space as well as pedestrian issues, staff has been particularly aware of the potential for adjacent sites, such as the Table Talk and Crate and Barrel, to redevelop, and has attempted to ensure that the proposed development is designed in such a way as to provide coordination with future ones. For example, there is the potential for an expanded open space area along Duke Street when the adjacent Table Talk property. At the request of the City, the applicant has agreed that the space will be privately maintained but publicly accessible.

In addition to a place where pedestrians, shoppers and nearby residents can sit, relax or simply enjoy an open area within the urban environment, the open space courtyard in this case provides an

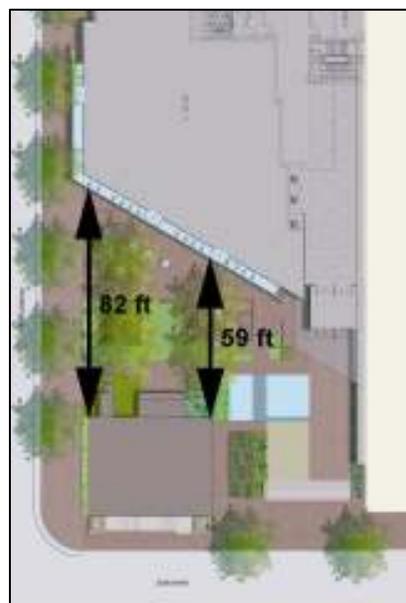


Figure 16: Proposed Open Space



Figure 17: Open Space Comparison: Carlyle House on left, proposed open space on right

important setting for the historic building being preserved at the corner of Duke Street and Reineker's Lane. By setting the new office building back significantly from the historic building (60-85 feet), the separation provides a small open space like area that frames the historic building, setting it apart from other buildings. In keeping with the respect and emphasis created by the distance between the new building and the historic one, staff is recommending that the design of the courtyard be refined to include an appropriate setting, including less hardscaped area and a more defined green space around the historic building.

In addition, the courtyard is a well-suited location for a memorial recognizing the history of the site and the historic building at 1707 Duke Street. The open space area as now proposed includes a commemorative space and a specific water feature, but alternative design concepts are currently the subject of discussions with neighborhood groups and the Black History Museum. Although the final design of the memorial and open space area has not been completed at this stage, staff has included conditions articulating specific design goals and objectives.

With the multiple purposes and opportunities of this open space courtyard in mind, staff has included recommendations to enhance its quality, to make the open space area more compatible with the existing and proposed buildings, and to provide an appropriate memorial to the historic site. These changes include:

- Increasing the amount of landscaping and trees in the open space area to form a courtyard around the historic building;
- Requiring that the open space/courtyard area consist of a portion with lawn or planted area;
- Creating an appropriate gathering area and memorial within the courtyard area to commemorate the solemn history of this site, such as a fountain or sculpture;
- Creating a clear separation between the courtyard area and any outdoor activity space of the new office building so that such things as outdoor dining can coexist harmoniously with the memorial area; and
- Requiring final design approval of the open space courtyard and memorial by BAR.

Finally, staff is also requiring that the applicant fund the archeological excavation of the site to further study and analyze the possible history of the site. Any artifacts found may be incorporated into the design of the open space area's memorial, or into the proposed development as a reminder of the historic importance of the site.

C. Building Mass and Scale:

A fundamental goal of the Master Plan is to guide development to ensure that the height, mass and density of development is appropriate within the context of the surrounding area and neighborhoods. The context of this proposal is slightly varied. Buildings immediately adjacent to the King Street Metro Station are of increased heights and density relative to the surrounding Old Town area. The development site, while close to the Metro, is actually within an intermediate area where buildings are of more moderate height and density than those closest to the station, yet still larger than the lower scale buildings to the east, in the heart of Old Town. It is important, therefore, that the proposed project be appropriately sized to help provide that important transition, while still allowing density consistent with its Metro proximate location.

In the King Street Metro area, building height is permitted up to 77 feet, and up to 82 feet with ground floor retail space. The building height is approximately 68 feet, or one-story less than the 77 foot limit permitted, and significantly less than the 82 feet permitted with ground floor retail, as is included here. On the other hand, in terms of density, the applicant is requesting special use permit approval to permit an FAR increase from 2.0 to 3.0, which is consistent with the surrounding floor area ratios in the area (see Figure 17). The additional floor area results in 36,691 sq. ft. of additional building area.



Figure 18: Floor Area Ratios

In addition to zoning requirements, and staff’s concern for adequate pedestrian connections and an ample open space area, staff reviewed the mass and scale of the project with two principal issues in mind. Of most significant concern is that the size, shape and design of the new building not overshadow the preserved historic building at the site’s south end. Additionally, staff worked to ensure a transition from the tall buildings adjacent to Metro to the proposed and future development further south and east of Metro.

The applicant has worked to address staff’s overall concern regarding the building mass as related to the historic house by creating a large setback between the historic house and the new building and by lowering the building height to five stories or approximately 68 ft. A clear, “floating” glass curtain wall is also proposed for the south facade, functioning as a transparent “veil” for the 100-year

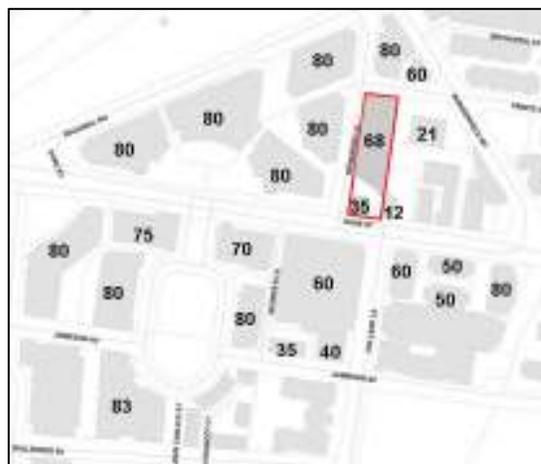


Figure 19: Building Height Map

old building. Staff has included conditions in its recommendation for approval to ensure that this potentially excellent solution be executed with the highest caliber material and design. Along Reineker's Lane, the building façade has been refined with increased modulation to reduce the perceived length of the building.

In addition, at staff's request, the applicant has included a setback of the fifth floor, to provide a better transition to the 84 ft., 81 ft. and 77 ft. tall buildings located to the west and north of the site, respectively. The applicant responded by providing building setbacks of 16 inches at the fifth level to help reduce the perceived mass and length of the building. The applicant has also agreed to incorporate higher-quality materials to assure that the details result in a first-rate building with a rich facade, especially at the pedestrian level. The applicant's request for an increase in the height of the mechanical penthouse is anticipated by the Zoning Ordinance at section 6-403(B) (2) (b), which limits its height to 15 feet, unless a special use permit is approved. In this case, the penthouse height is 18 feet, and required, in part, because of the shallowness of the property, east to west, and the increased setback/open space area on the south. The compressed roof area requires taller mechanical appurtenances.

Finally, staff is recommending several additional architectural refinements to further reduce the perceived length and mass of the building. With the applicant's revisions and staff's recommendations, the development can achieve a mass, scale and orientation which reflect the quality of high density mixed-use development near the King Street Metro station that the Small Area Plan requires. It is for these reasons that staff supports the proposed FAR increase to 3.0 and the small increase in penthouse height.

D. Building Design:

Consistent with the Small Area Plan, the massing and orientation of the proposed Edmundson Plaza building reflects quality, high density mixed use development near the King Street Metro Station. The applicant has worked with staff to address staff's overall concern with the building mass as related to the historic house by creating a large setback between the historic house and the new building, and also by lowering the building height. An angled, clear, "floating" glass curtain wall is also proposed for the south facade, functioning as a transparent backdrop to the 100-year old building. Along Reineker's Lane, the building facade has been refined with increased modulation and setbacks to reduce the perceived length and mass of the building.

It is important that the building design incorporate high quality materials and detailing, not only because of the prominent location of the site, but also to help reduce the perceived mass of the building as a backdrop to the historic house. Staff supports the design approach of a contemporary, brick office building with predominately punched window opening expression, and metal- and glass-clad accent bays. The angled portion features a glass curtain wall with an outer layer of clear, butt-jointed glazing, designed to appear to float in front of a second layer of slightly less transparent curtain wall glazing. Staff recommends the use of a larger spacing

between mullions in this backdrop element, or introduction of one-way butt-jointed glazing, especially in the retail façade, both to create a less distracting backdrop to the historic house, and also to enhance the visibility, viability, and transparency of the retail space along Reineker’s Lane and facing the proposed courtyard. Staff is also recommending additional refinement to the proposed northern bays and treatment to the top of the building.

E. Bulk Plane/Setback Modification:

Although the OCH zone does not have yard requirements, the Zoning Ordinance requires that the building be set back from the centerline of the street it faces a distance equal to at least half the height of the building at every point. In this case, one key factor warrants a modification of this technical requirement along Prince Street and a portion of the building along Reineker’s Lane. The early Metro development of the King Street Station Shops complex and the building at the intersection of Diagonal and Daingerfield Roads “shifted” the roadway at this location resulting in an offset alignment of the road.

The height of the building along Prince Street varies due to the grade change of the street sloping down about four feet from the intersection of Reineker’s Lane to the eastern edge of the property. Along Prince Street, the proposed building varies in height from 66 to 70 feet, with a setback of 32 feet from the centerline. The setback rule requires a setback of 33 to 35 feet, resulting in a requested modification of 1 to 3 feet. Along Reineker’s Lane, the portion of the building that does not meet the setback requirements is 72 feet tall with a 33.5-ft. setback from the centerline. The setback rule requires a 36-ft. setback from the centerline of Reineker’s Lane, and therefore a 2.5-ft. modification is required.

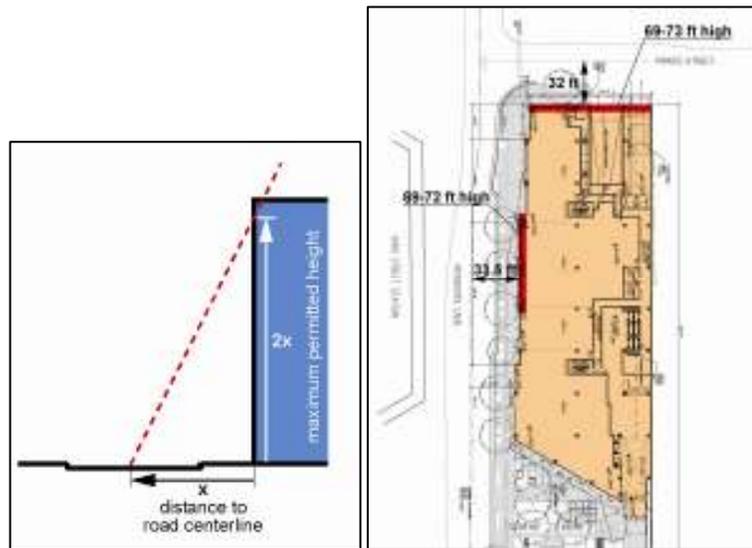


Figure 20: Setback Modifications

The intent of this provision was to ensure an appropriate distance between buildings on each side of the street. Because the street was shifted as part of the King Street Station development this site now requires a modification. However, the distance between the proposed building and the

buildings across the street on Reineker's Lane will generally be between 60 and 71 feet, which meets the intent of the requirement. For this reason, staff recommends approval.

F. Uses:

Office Use:

The City of Alexandria's Economic Sustainability Workgroup has indicated that the City needs a more concentrated focus on attracting businesses to the area. Their preliminary findings state:

- Alexandria has assets that are sound and that can be identified and leveraged for greater benefit;
- Job growth has been relatively small in the City when compared to the job boom in the rest of northern Virginia;
- Alexandria is at risk of becoming an urban bedroom community and less of a major job center;
- The economic viewpoint is not a regular part of the decision making process;
- Regional competition for consumer spending is getting tougher;

The Economic Sustainability Workgroup also found the following:

- Metro stations are underdeveloped and have substantial future potential;
- The City needs to set job growth goals and apply them in land use decision-making;
- Alexandria needs to focus on commercial businesses where they can add unique value.

The proposed office building, near the King Street Metro Station, offers an opportunity to provide a high-quality development that will further the goals of providing more intense commercial development near transit nodes, while providing needed office space, as well. This development is consistent with the goals outlined by the Economic Sustainability Workgroup, and will balance the primarily residential development in the vicinity.

Retail Uses:

The proposed building includes 15,800 sq. ft. of retail/restaurant uses on the ground floor facing Reineker's Lane. The two tenant spaces at each end wrap around the corners of the building so that there are also windows and doors facing Prince Street and the outdoor courtyard near Duke Street. The retail and restaurant space is an important element of this development that will bring activity to the street level, while providing services to neighboring residents and office workers alike. Staff supports these uses since they will bring energy and vitality to Reineker's Lane, which is currently an underutilized area within walking distance to the King Street Metro station. These uses also complement the retail area on the south side of Duke Street and the other restaurants in the area.

While retail uses can bring activity and life to an area, staff is aware that retail space, unless occupied by strong, pedestrian supportive tenants can also be a challenge. On the other hand, this particular site has many attributes in favor of successful retail. It is a highly visible location. The design of the building is dramatic and the open space area inviting. Staff has inspected the area and found surprisingly high concentrations of pedestrian traffic now walking along Reineker's Lane to and from the Metro. Staff assumes that the advent of the Whole Foods grocery store has created new pedestrian travel routes, and that, coupled with nearby restaurants, and Carlyle, the site's location at the crossroads of several routes should work to make this retail area succeed.

Staff does have concerns regarding the nature of the retail to be located at the site, particularly with regard to those retail spaces adjacent to the open space courtyard, which will house a contemplative memorial commemorating a solemn time in African American history of the City. Staff is concerned that, given the breadth of the retail and personal service uses permitted by the zoning ordinance, certain permitted uses could detract from the significance of the property. Staff does not have the authority to limit specific uses to ones the Director finds appropriate on a case-by-case basis or to require a special use permit for those uses the Director finds inappropriate.

Restaurant Use:

The applicant has indicated that a portion of the retail space may be used as a full-service, sit down restaurant and is requesting SUP approval for the use with this development application. Staff supports request since restaurants, including restaurants with outdoor dining, are an important element in creating a vibrant and active streetscape. A restaurant at this location is consistent with other restaurant uses in the area, including La Porta on Duke Street and Joe Theisman's on the northern corner of Reineker's Lane at Diagonal Road.

The applicant does not yet have details regarding the floor area, number of employees, and number of tables that will be part of the restaurant space. Nor does the applicant know the future operator. Therefore, staff recommends that approval of one full service restaurant with standard conditions for a full-service, sit down restaurant use, be included as part of this development approval. An administrative approval can be processed (change of ownership) at a later date when an operator is chosen to identify the details and to assure compliance with the requirements of this permit, which are fashioned from the Carlyle SUP, and include the following:

- Restaurants shall close no later than 12:00 a.m.
- All patrons shall be seated by a host or hostess, printed menus shall be provided at the tables, service shall be provided at the tables by a waiter or waitress, and tables shall be preset with non-disposable tableware and glassware.
- Deliveries to the business shall not take place between the hours of 7:00 a.m. and 9:30 a.m., or between 4:00 p.m. and 6:00 p.m., Monday through Friday. Deliveries to

restaurants within residential buildings or whose service drive adjoins a residential building shall not take place before 7:00 a.m. or after 10:00 p.m.

- If any food delivery services are provided, they shall clearly be accessory to dine-in food sales, and all deliveries shall be non-vehicular (made on foot via bicycle, etc.). Alcoholic beverages shall not be delivered off-site, and delivery of nonalcoholic beverages shall only be in association with food deliveries.
- Alcoholic beverages may be sold for on-premises consumption only, and shall clearly be accessory to food sales.
- All outdoor dining areas shall be accessory to an approved indoor restaurant.

Any additional restaurant proposed for the retail space will require full SUP approval.

Health Club:

The proposal includes a 12,000 sq. ft. health club within the first underground level of the building. The health club use is permitted with an SUP within the OCH zoning district. Although the health club counts toward the on-site parking requirements for this development, the below grade space devoted to the use is not included in the overall floor area ratio calculation since it is below grade. The applicant is unable to supply sufficient detail regarding the use, such as the operator, hours, and type of health club, for staff to be comfortable with the use at this preliminary point. Unlike a restaurant, whose characteristics are well known, even given variations in some features, health clubs can vary significantly as to peak times, and especially as to parking requirements, depending on the detailed proposal and operator. Staff therefore recommends that the health club use not be approved with this development permit. Rather, it should be considered for approval as a separate SUP, pending more information regarding the applicant, number of employees, number of required parking spaces, and how the parking will be accommodated.

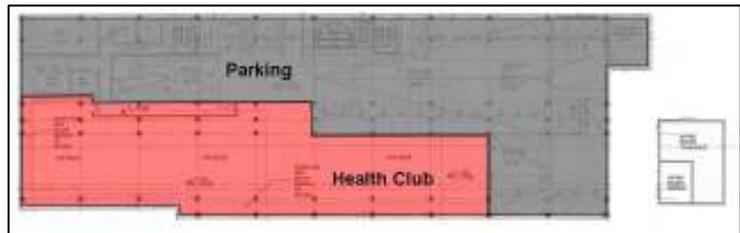


Figure 21: Health Club Floor Plan

G. Demolition of Existing Buildings:

The existing 2,745 sq. ft., 100-year old historic structure at 1707 Duke Street will be retained on the site with an open space/outdoor courtyard area surrounding the building. While this structure will remain, two other non-historic buildings will be demolished to allow this development, including a non-historic, 1950's era building and a building on Duke Street currently used for a travel agency. The 1,016



Figure 22: Building to be demolished on Duke Street



Figure 23: Building to be demolished on Reineker's Lane

sq. ft. building facing Reineker's Lane is a brick building with a 3,478 sq. ft. brick and cinderblock building extension in the rear. The 3,399 sq. ft. building on Duke Street currently being used as a travel agency is proposed to be demolished to allow for a portion of the open space to connect to Duke Street, and to allow the office building main entrance to face Duke Street.

H. Green Building/LEED Certification:

At the direction of City Council, the Planning Commission, and as articulated in Council's Strategic Plan, staff has been incorporating various green and sustainable site and building elements into each development proposal in recent years. This approach has been very effective in providing considerable benefit to the City. As the Planning Commission and Council are aware, staff is evaluating elements such as cost and certification methods for commercial and residential uses and plans to begin meeting with members of the development community and residents. It is staff's goal that by fall 2007, it will be prepared to propose a potential city-wide policy for new developments.

In the meantime, staff has been working to implement the certification processes for cases such as the proposed North Payne St. residential project and the Fire Station/affordable housing development in Potomac Yard. Staff strongly encouraged the applicant to incorporate site and building elements that would enable the building to achieve 23 points necessary for a LEED certifiable building. The Leadership in Energy and Environmental Design (LEED) building rating system represents the U.S. Green Building Council's effort to provide a national standard for what constitutes a "green building."

I. Vacation of a Portion of Reineker's Lane:

The applicant is requesting approval of a vacation to permit the two levels of the underground parking garage to extend into the area below the adjacent public right-of-way on Reineker's Lane. As with other modifications and special requests in this development application, this one is the result of the awkward shape, size and configuration of the subject property. Specifically, an area approximately 12 feet in width, for a depth of approximately 32 feet, and a length of the 125 feet, will be located beneath the Reineker's Lane right of way. The proposed vacation space will be used for parking spaces within the underground garage.



Figure 24: Proposed Vacation

Staff is recommending approval of the proposed vacation of approximately 910 sq. ft. for the underground garage. While not ideal, the vacation is needed due to the narrow width of the site. While staff is recommending approval, the approval is contingent on the following:

- Provision of fair market value for the land to be vacated.
- Public accessibility of the sidewalk.
- City retention of subsurface rights for any future utilities.
- The vacation shall be limited to the underground garage.
- The vacated area shall not be permitted for density-floor area.

With these recommendations, the City will continue to have the right to use the sidewalks and area for below grade utilities, while also allowing this proposal to proceed.

J. Parking:

The proposed development is located within the King Street Transit Parking District, which allows reduced parking requirements to encourage transit ridership. Specifically, parking for office use may be allowed at a ratio of one space for each 665 sq. ft. of development with approval of a special use permit. In addition, the first 10,000 sq. ft. of retail/restaurant space is exempt from the parking requirements. This approach to encourage transit use and reduce parking around the City's Metro stations has been a long standing City policy and is consistent with the parking strategy for the recently adopted Eisenhower East Small Area Plan. That Plan establishes *maximum* parking requirements rather than minimum parking requirements.

In this case, the applicant is requesting approval of the reduced office parking ratio. The proposed parking complies with the reduced ratio for office, and sufficient parking is provided to meet the other parking requirements of the proposal. Based on the intent of encouraging transit ridership, and the comparable amount of parking provided within adjoining office buildings/uses, staff is recommending approval of the proposed lower parking ratio, contingent on the following:

- Size/Configuration of the Spaces;
- Shared Use of the Underground Garage; and
- Market Rate Parking For Office Uses.

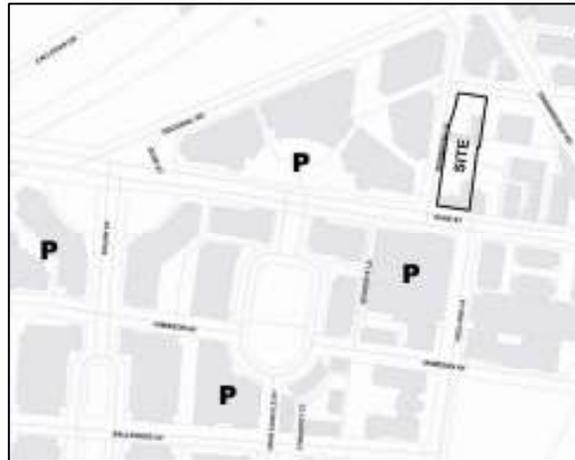


Figure 25: Public Parking Locations

Size/Configuration:

The proposal includes 179 total parking spaces, including 136 standard size spaces, 37 compact spaces, and 6 handicap spaces.

Shared Use of the Underground Garage:

Staff is recommending that the proposed parking garage be available to the general public after normal office hours to accommodate parking needs in the area. Staff has also included conditions requiring the construction of the garage to contain knock-out panel(s), a public access easement and a design that accommodates future access, allowing for the potential for a joint parking garage should the properties to the east be redeveloped.

Market Rate Parking:

Staff is recommending a condition that requires the parking charges for the underground parking to be at market rates comparable to other buildings within the City of Alexandria. However, staff recommends that free parking be provided for short-term visitor parking. Staff also recommends that all office employees be required to pay market rates for parking, and that no parking be provided free or at reduced rates or with costs reimbursed by the employer, unless the employer provides an equivalent benefit to all employees who utilize transit options to commute.

K. Mass Transit Incentives:

Traffic Impact Study:

A Traffic Impact Study (TIS), dated March 27, 2007, and completed by Wells & Associates LLC, indicates that the Duke Street corridor is well served by transit, including both bus and rail service with connections to the metropolitan area. The TIS indicates that the proposed office space, ground-floor retail space, and health club would generate 161 AM peak hour trips and 265 PM peak hour trips, and 2,389 daily trips when the project is complete and at full occupancy, assuming the 35% transit reduction for office uses and the 25% transit reduction for the retail uses.

The study indicates that the roads in the surrounding area currently operate at acceptable levels of service during both the AM and PM peak hours, with the exception of the southbound approaches at Duke Street/Reineker's Lane and Duke Street/Daingerfield Road and the eastbound approach at Daingerfield Road/Prince Street. According to the TIS, only signal optimization at the intersection of Duke Street/Reineker's Lane will be required to maintain or improve existing levels of service and the site driveway on Prince Street should operate at acceptable levels of service during both the AM and PM peak hours.

Staff recommends that the TIS be amended and updated to include the entire health club use, if expanded, when the future SUP application is filed, and that it take into consideration the specific floor space, number of employees, number of parking spaces, hours of operation, of the

particular health club anticipated. Any additional conditions relating to this SUP should be made part of that SUP consideration.

Transportation Management Plan/Mass Transit Incentives:

The Transportation Management Plan (TMP) (#2007-0018) indicates that the site will be served by many forms of transit services, including: bus service, Metrorail, Virginia Railway Express (VRE), DASH, and Richmond Highway Express. The TMP has a goal of 25% non single-occupant vehicle travel during the peak time periods. In order to accomplish this goal, the TMP indicates the following:

- A car share program will be established and marketed as part of the ridesharing and transit marketing efforts for the building;
- Information about transit, ridesharing and other TMP elements shall be distributed and displayed to employees;
- A rideshare program will be established;
- Discounted bus and rail fare shall be sold on-site to office and retail workers;
- Establish a TMP Coordinator position with experience in this occupation to improve and manage all aspects of the TMP;
- Transit, ridesharing, staggered work hours/compressed work weeks, parking restrictions, the elements of the parking management plan and other program elements shall be promoted to prospective occupants;
- A guaranteed Ride Home Program will be established and promoted as part of the ridesharing and transit marketing efforts; and
- Bicycle racks shall be provided in quantities sufficient to meet demand.

L. Affordable Housing:

The applicant will make a voluntary contribution to the Housing Trust Fund in an amount consistent with the recommendations of the Developer's Housing Contribution Policy Work Group (DHCPWG). Specifically, the applicant's contribution will follow the accepted formula of \$1.50 per gross square foot. The applicant has agreed to make a contribution to the City in the amount of \$191,862, which is consistent with the DHCPWG.

VI. COMMUNITY:

Staff and the applicant conducted 10 community meetings for this project with the Upper King Street Civic Association, Old Town Civic Association, Old Town Business and Professional Association, the Royalton Condominium Association, the Metropolitan Condominium Association, and the Alexandria Black History Museum. The main issues identified by the community include:

- Minimizing the traffic impacts and making the street more pedestrian friendly;
- Ensuring the mass and scale of the building is compatible with the neighborhood;
- Providing active ground-floor uses, particularly fronting on the open space courtyard;
- Minimizing the visibility and noise of the loading/service area along Prince Street;
- Communicating and commemorating the past of the historic house and the historic events related to the house; and
- Garage and loading access.

The development process has included extensive community feedback. There are numerous changes and revisions to the proposal that have improved the plan through active citizen involvement.

VII. CONCLUSION:

Staff recommends **approval** subject to the attached list of conditions.

STAFF: Faroll Hamer, Director, Department of Planning and Zoning;
Jeffrey Farner, Chief, Development;
Bob Leeman, Principal Planner; and
Natalie Sun, Urban Planner.

VIII. STAFF RECOMMENDATIONS/CONDITIONS

Staff recommends **approval** subject to compliance with all applicable codes and ordinances and the following conditions:

A. PEDESTRIAN/STREETSCAPE

1. The applicant shall provide pedestrian-streetscape improvements that at a minimum shall provide the level of improvements depicted on the preliminary site plan and shall also at a minimum provide the following to the satisfaction of the Directors of P&Z and T&ES:

Duke Street

- a. The applicant shall install two pedestrian crosswalks at the intersection of Duke Street and Reineker's Lane as generally depicted in *Attachment #1*, and at a minimum consist of the following:
 - i. The central median on Duke Street shall be reduced in length by approximately 10 ft to provide a continual 10 ft wide pedestrian crosswalk;
 - ii. City standard black pedestrian count down signals and poles;
 - iii. Relocation of the cobrahead light on the south side of Duke Street to the mast arm or outside the pedestrian ramp-walkway;
 - iv. A stamped and colored asphalt crosswalk on Reineker's Lane; and
- b. To provide a wider sidewalk on Duke Street and additional street trees, and to maintain visibility of the 100-year old building, the applicant shall relocate the existing cobra head light to provide the following:
 - i. The existing cobra head along the north frontage of Duke Street shall be incorporated as part of the existing mast arm along the frontage on the north side of Duke Street.
 - ii. Shift the proposed street trees to be approximately 25 ft. on-center to maintain a minimum 10 ft. unobstructed sidewalk on each side of the proposed street trees and in relation to the historic house at 1707 Duke Street, as generally depicted in *Attachment #1*.

Reineker's Lane and Prince Street

- c. A landscape strip shall be provided for the first three street trees along Reineker's Lane near the corner of Duke Street as generally depicted in *Attachment # 1*. The proposed street lights shall be located to the satisfaction of the Directors of P&Z and T&ES.
- d. The Applicant shall install two (2) above ground planters over the 230 KV line on Reineker's Lane near Prince Street for street trees if approved by Dominion Virginia Power.

- e. The brick sidewalk on Prince Street shall continue over the garage ramp and loading dock entrance at the same level of the sidewalk to provide a continual uninterrupted brick sidewalk.
- f. The brick sidewalk shall be extended to the east to the existing curb cut for the Crate & Barrel property, including the provision of an additional street tree to the east of the loading area.
- g. Two colored and stamped crosswalks shall be provided at the intersection of Prince Street and Reineker's Lane extending in each direction.
- h. A trash can shall be provided at the intersection of Reineker's Lane and Prince Street.
- i. The minimum width of the sidewalk shall be 14 ft wide as generally depicted on the preliminary site plan.

General:

- j. All transformers shall be located within an underground vault. Transformers shall not be located within the sidewalk or open space/courtyard areas. The applicant shall be responsible for installing and maintaining the underground vault.
 - k. The new mast arm and pedestrian countdown signals shall be City Standard matte black finish.
 - l. All perimeter street lights shall be City standard "Gadsby" street lights.
 - m. All sidewalks, including the internal courtyard sidewalks shall be standard red brick and shall comply with City standards.
 - n. The walkways for the internal sidewalks shall generally be the same brick as used for the perimeter sidewalks to reinforce the public nature of the space.
 - o. All pedestrian improvements shall be completed prior to the issuance of a certificate of occupancy permit. (P&Z) (T&ES)
2. The applicant shall contribute \$1.10 per square foot of gross floor area toward the King Street Metro Area Improvement Fund prior to the release of the final site plan. The applicant shall receive a credit for the reconfiguration of the median on Duke Street, the relocation of the existing cobra head, street light modifications, the installation of stamped and colored crosswalks, the reconfiguration of the stairs in front of the historic building, the extension of the sidewalk and installation of the additional street tree along Prince Street beyond the property line, and the provision of benches and planters along Reineker's Lane. (P&Z) (T&ES)
3. The applicant shall provide all pedestrian and traffic signage in accordance with the *Manual of Uniform Traffic Control Devices (MUTCD)*, latest edition, to the satisfaction of the Director of T&ES. Pedestrian and traffic signage shall be depicted on the final site plan, installed by the developer to the satisfaction of the Director of T&ES. Signposts shall be 2" diameter galvanized poles painted black; signs shall be Highway C aluminum, 0.080 gauge blank, 3M VIP sheeting. (T&ES)

4. All ADA ramps shall be concrete with truncated domes on the end of the ramp with contrasting color from the rest of the ramp. (T&ES)
5. The Applicant shall provide \$1,000/ea to the Director of T&ES for the purchase and installation of City standard street cans along the public streets and within the open space area for each door that provides access into the project to the satisfaction of Directors of T&ES and P&Z. The applicant shall provide no less than eight (8) and no more than eleven (11) trash cans. All trash cans shall be Iron Site Bethesda Series, Model S-42 decorative black metal trash cans. (T&ES)(P&Z)

B. OPEN SPACE – LANDSCAPING:

6. The final design of the open space plaza, the landscaping adjacent to the 100-year old building, and the redesign of the stairs and planters in front of the 100-year old building is subject to the approval of the Old and Historic Board of Architectural Review. The owner of the 100-year old building will obtain the approval of the Old and Historic Board of Architectural Review for any improvements related to the 100-year old building. The Applicant shall be responsible for obtaining the Old and Historic Board of Architectural Review approval for the treatment and design of the open space plaza. (P&Z)
7. The open space area on the southern portion of the site shall provide amenities such as special paving surfaces, materials, benches, trash receptacles, landscaping, etc. to encourage its use to the satisfaction of the Directors of P&Z and RP&CA. In addition, the open space shall reviewed and approved by the Old and Historic Board of Architectural Review:
 - a. The open space/courtyard shall be fully open to the public following the hours and guidelines established by the Department of RP&CA or additional hours if requested by the Director of P&Z for events or use(s) of the open space. The applicant shall also coordinate with the City to have special events and/or tours commemorating the history of the site.
 - b. At such time as the adjoining Table Talk site (Tax Assessment Map #: 073.02-02-07) is redeveloped, the applicant shall coordinate with the City and the adjoining developer and/or property owner to ensure that the two open space areas are connected visually and physically, and so that pedestrians can access both areas easily, including possible revisions to the open space and connections. The cost of any physical revisions to the open space design shall be the responsibility of the developer of the adjoining site.
 - c. A public focal element such as a sculpture or fountain that is an appropriate scale for the space shall be provided and permanently mounted within the open space. Provide, at a minimum, two decorative benches and one trash receptacle in the courtyard open space area of the site.

- d. Where walls or planters are necessary they shall be brick or stone.
 - e. The proposed wall on the eastern property line shall be eliminated and replaced with landscaping at such time as the Table Talk site (Tax Assessment Map #: 073.02-02-07) is redeveloped.
 - f. A freestanding sign for the proposed office building and/or retail/restaurant shall be prohibited.
 - g. The courtyard and 100-year old building shall include lighting of the courtyard, proposed building and 100-year old building. (P&Z) (RP&CA)
8. A final landscape plan shall be provided with the final site plan to the satisfaction of the Directors of P&Z and RP&CA. The plan shall include the level of landscaping depicted on the preliminary landscape plan and shall at a minimum also provide:
- a. The type of street trees proposed on Reineker's Lane, Duke Street, and Prince Street shall be determined during Final Site Plan.
 - b. The applicant shall provide an evergreen groundcover and shrub plantings within the Duke Street median.
 - c. The perimeter street trees and trees within the open space courtyard shall be a minimum of 4 inch caliper at the time of installation.
 - d. An additional street tree shall be provided to the west of the proposed garage entrance on Prince Street.
 - e. All street trees shall be planted in a 4 ft. wide continuous planting trough with aeration, drainage, and irrigation systems. Where on-street parking is not provided, such as adjacent to the open space/courtyard, the trees shall be planted within a continual 5ft. wide landscape planting strip.
 - f. Foundation plantings and shrubs shall be provided adjacent to the 100-year old building on Reineker's Lane.
 - g. Evergreen groundcover shall be provided within each tree well and reflected in the plant schedule.

General

- h. The soil depth and arable volume on top of the parking deck shall comply with the new City of Alexandria Landscape Guidelines.
- i. Provide crown coverage area calculations in compliance with the City of Alexandria Landscape Guidelines, or provide updated crown coverage calculations.
- j. The garage exhaust and/or intake shall not be located within the sidewalk or open space/courtyard areas.
- k. No shrubs higher than three feet shall be planted within six feet of walkways. All landscaping shall be maintained in good condition and replaced as necessary.
- l. Utility lines such as water, storm sewer and electric lines shall be located to minimize impacts on proposed street trees and open space.
- m. The location of all light poles shall be coordinated with the street trees.
- n. Landscaping shall be maintained by in good condition and replaced as needed.

- o. All plant materials and specifications shall be in accordance with the current and most up to date edition of the American Standard for Nursery Stock (ANSI Z60.1) as produced by the American Association for Nurserymen, Washington, D.C.
 - p. All trees to be limbed up a minimum of 6 ft as they mature to allow for natural surveillance.
 - q. At a minimum the Landscape Plan shall be prepared and sealed by a Landscape Architect certified to practice in the Commonwealth of Virginia.
 - r. Landscaping notes and graphic symbols for trees are to be provided in accordance with City of Alexandria Guidelines.
 - s. Provide correct botanical name for street trees and provide City of Alexandria Street Tree Planting detail.
 - t. Provide note on drawings which indicates that in the absence of more strenuous specifications, plantings will be installed in accordance with the latest and most current edition of Landscape Specification Guidelines, as produced by the Landscape Contractors Association (LCA) of Maryland, District of Columbia and Virginia, Gaithersburg, Maryland.
 - u. Provide detailed planting plans at a scale of at least 1/8 inch equals one foot, for entrance facades of each building:
 - i. All shrubs shall be installed at a maximum of 30 inches on-center spacing installed at a minimum size of 24 inches. Adjust quantities accordingly.
 - ii. Clearly show limits of planting beds and grass areas.
 - iii. All grass areas shall be specified as grass sod.
 - iv. Do not block or compromise FDC connections with plantings.
 - v. Provide street tree planting details.
 - vi. Street trees and trees associated with open space design shall (at request and recommendation of applicant during Concept Review/Response Phase for project development) be 4 feet in vertical depth and provide 1,000 cubic feet of planting soil per individual tree. A detail shall be provided on the final landscape plan. The detail on the preliminary plan shows perforated PVC aeration pipe that appears to be installed horizontal at surface of planter, which should be corrected.
 - vii. Provide detail sections including drains for plantings above structure.
 - viii. Provide location of underground garage and site utilities on landscape plans.
 - ix. Provide matching street trees in vacant planters along Reineker's Lane and adjacent to building column lines B and C. (P&Z) (RP&CA)
9. The applicant shall provide an irrigation plan and details for the landscape plan that is developed installed and maintained to the satisfaction of the Director of RP&CA which shall at a minimum consist of the following:
- a. Information shall include location and type of heads/emitters, water delivery

- systems, sleeving beneath pavement/roads/sidewalks, controller(s), backflow preventer(s) and all system monitoring devices.
- b. Irrigation plan shall be prepared and sealed by an Irrigator with certification at a level commensurate to this project and licensed to practice in the Commonwealth of Virginia.
 - c. Provide external water hose bibs continuous at perimeter of building. Provide at least one accessible external water hose bib on all building sides at a maximum spacing of 90 ft apart.
 - d. Hose bibs and ground set water connections must be fully accessible and not blocked by plantings, site utilities or other obstructions. (RP&CA)

C. USES: RESTAURANT – RETAIL – HEALTH CLUB

- 10. a. A full-service restaurant with outdoor seating may be approved administratively by the Director of P&Z, provided that any restaurant approved under this provision complies with the conditions listed below. Restaurants that do not meet these criteria shall be subject to subsequent special use permit approval.
 - i. Restaurants shall close no later than 12:00 a.m.
 - ii. All patrons shall be seated by a host or hostess, printed menus shall be provided at the tables, service shall be provided at the tables by a waiter or waitress, and tables shall be preset with non-disposable tableware and glassware.
 - iii. Deliveries to the business shall not take place between the hours of 7:00 a.m. and 9:30 a.m., or between 4:00 p.m. and 6:00 p.m., Monday through Friday.
 - iv. If any food delivery services are provided, they shall clearly be accessory to dine-in food sales, and all deliveries shall be nonvehicular (made on foot via bicycle, etc.). Alcoholic beverages shall not be delivered off-site, and delivery of nonalcoholic beverages shall only be in association with food deliveries.
 - v. Alcoholic beverages may be sold for on-premises consumption only, and shall clearly be accessory to food sales.
 - vi. Kitchen equipment shall not be cleaned outside, nor shall any cooking residue be washed into the streets, alleys, or storm sewers.
 - vii. No food, beverages, or other material shall be stored outside.
 - viii. Trash and garbage shall be placed in sealed containers, which do not allow odors to escape and shall be stored inside or in a closed container, which does not allow invasions by animals. No trash or debris shall be allowed to accumulate on-site outside of those containers.
 - ix. Litter on the site and on public rights-of-way and spaces adjacent to or within 75 feet of the premises shall be picked up at least twice a day and at

- the close of business, and more often if necessary, to prevent an unsightly or unsanitary accumulation, on each day that the business is open to the public.
- x. Cooking odors, smoke and any other air pollution from operations at the site shall be properly ventilated and shall not be permitted to become a nuisance to neighboring properties, as determined by the Department of T&ES.
 - xi. The Applicant shall contact the Crime Prevention Unit of the Alexandria Police Department for a security survey and a robbery awareness program for all employees.
- b. Restaurants – Outdoor Dining: Outdoor dining is encouraged and may be permitted administratively by the Director of P&Z subject to the following minimum criteria and conditions:
- i. All outdoor dining areas shall be accessory to the indoor restaurant.
 - ii. An unobstructed pathway with a minimum width of 10 feet shall be provided at all times.
 - iii. Any permanent structures which are required for the outdoor seating area shall be subject to review by the Director of P&Z.
 - iv. Live entertainment shall be permitted in the open space plaza near the outdoor seating area provided that the applicant contacts both the adjacent residential neighbors and civic associations and obtains the approval of the Director of P&Z.
 - v. No sound amplification shall be permitted in the outdoor seating area.
 - vi. Any outdoor seating areas shall not include advertising signage. The design of the outdoor seating shall be compatible with the design of the building.
 - vii. On site alcohol service shall be permitted; no off-premise alcohol sales are permitted.
 - viii. No food, beverages, or other material shall be stored outside.
 - ix. The applicant shall provide, at its expense, one city trash container Model SD-42- exclusively for the outdoor dining area. The trash container shall be emptied at the close of business each day. (P&Z) (T&ES)
11. The ground floor retail tenant spaces, as depicted on the Preliminary Plan dated March 23, 2007, shall be solely utilized by retail uses including: a store engaged in the sale of goods for personal use that shall include bakeries, barber shop/beauty salon, banks, bookstores, clothing, clothing accessories, copier/reproductions, department stores, drugstores, dry cleaners (not dry cleaning plant), florists, cigar shops, travel agencies, health and sport clubs, groceries, jewelry, restaurants and any similar uses deemed by the Director of P&Z to meet the intent of providing active pedestrian-oriented neighborhood-serving retail uses. The floor-to-floor height of the retail space shall be a minimum of 16 ft. (P&Z)

12. The proposed health club use shall require subsequent special use permit approval by the Planning Commission and City Council. (P&Z)
13. Exhaust vent shafts and grease traps shall be located within the retail space to accommodate the ground floor restaurant uses. (P&Z)
14. Professional offices may be permitted in the designated retail spaces but shall occupy no more than 50% of the gross floor area and shall be located on the Prince Street and Reineker's Lane portion of the building. (P&Z)

D. BUILDING

15. The massing, articulation and general design of the office building shall be generally consistent with the drawings and renderings submitted with this application dated May 18, 2007. The final design of the building shall be revised to incorporate the following to the satisfaction of the Director of P&Z:
 - a. With the exception of the metal base elements at the retail level, insulated metal panel system for the projecting accent bays along the west and north facades, windows and the feature glass curtain wall, the building shall consist of entirely masonry (brick, precast, stone) materials for the facade, including the screening of the mechanical equipment.
 - b. The glass feature wall shall be designed to incorporate two related but distinct glass façade layers, the outer one consisting of four-way structural glazed butt-jointed elements, with high transparency and low reflectivity (clear low iron/low E). Its edges shall be detailed to reinforce the impression of a floating plane. The inner layer shall be detailed to serve as an effective backdrop to the outer layer, with two-way structural glazing or expressed mullions, and transparency/reflectivity characteristics that set it apart from the feature layer (clear low E glass with a slight tint). Window sizes at the retail level of the glass feature element shall be revised to be comparable to the treatment on Reineker's Lane. Window coverings in the glass feature element shall be uniform and managed by the building owner to create and maintain a consistent effect.
 - c. An approximately 10 ft wide x 12 ft high mock-up of the glass feature wall shall be constructed on the southern portion of the site, of sufficient size to show the character of the glass wall, and shall be approved by the City Architect prior to ordering the glass.
 - d. The bays on the northern façade shall be refined to relate to the treatment on the western facade. The color of the loading door shall match the adjacent wall material and shall be integrated into the surrounding facade to minimize its visual prominence.
 - e. The cornice treatment at the top of the fourth floor along Reineker's Lane shall be

- restudied to create a stronger horizontal expression line.
- f. A strong vertical reveal shall be incorporated at the southeast and northeast corners where the office block meets the core with glazing, recessed brick, or metal louver, to better integrate these two forms.
 - g. HVAC units and mechanical appurtenances shall be located on the roof-tops, recessed and screened from view from the public streets.
 - h. The windows shall promote visibility with high ceilings and windows in clear glass.
 - i. The building shall provide low-level lighting for the new building and the 100-year-old building as an integral part of the facade design to add nighttime visual interest to the buildings. Accent lighting is encouraged.
 - j. All refinements to the design and materials shall be to the satisfaction of the Director of P&Z prior to the release of the final site plan. (P&Z)
16. The applicant shall hire a LEED certified consultant as a member of the design and construction team. The consultant shall work with the team to incorporate sustainable design elements and innovative technologies into the project and shall achieve 23 points towards LEED Core and Shell certification under the U.S. Green Building Council's systems. The architect shall provide a checklist and specific examples prior to the release of a building permit, to the satisfaction of the Directors of P&Z and T&ES. (T&ES)(P&Z)
17. The applicant shall be permitted to refine the north façade (Prince Street) and the mechanical penthouse to the satisfaction of the Director of P&Z. (P&Z)
18. The applicant shall provide plan and section details of the conditions adjoining the garage and loading dock doors. Both doors shall be an opaque screen or screen to minimize the projection of light from the garage onto the adjoining street. (P&Z)
19. The colors and materials of the retail and/or office tenant signs shall be designed of high quality materials to the satisfaction of the Director of P&Z and shall comply with applicable codes and ordinances as well as the following guidelines:
- a. Sign messages shall be limited to logos, names and street address information.
 - b. Wall signs above the first level for retail and/or office uses and illuminated signs above the first level for retail and/or office uses are prohibited on Duke Street.
 - c. Signs applied to storefront windows shall cover no more than twenty percent of the glass.
 - d. Box signs shall be prohibited.
 - e. Permanent or temporary advertising banners shall be prohibited.
 - f. Storage cabinets, carts, window signs, posters, shelving, boxes, coat racks, storage bins, closets, and similar items shall not block the visibility of the interior of the store from the street. This condition, however, is not intended to prevent retailers

- from displaying their goods in display cases that are oriented towards the street frontage.
- g. All window coverings shall be open as much as possible and provide some interior accent lighting when the business is closed.
 - h. Temporary freestanding signs for the purpose of marketing the development shall be allowed to the satisfaction of the Director of P&Z.
 - i. Window signs or posters shall not block the visibility of the interior of the store from the street. Shelving, boxes, coat racks, storage bins, closets, and similar items shall be located where they do not block the windows. All window coverings shall be open as much as possible and provide some interior accent lighting when the business is closed.
 - j. The colors and materials of signs shall be designed to be integrated into the architecture of the building and relate in materials, color and scale to the building.
 - k. Sign messages shall be limited to logos, names and street address information.
 - l. Freestanding signs other than traffic/directional signs shall not be permitted. (P&Z)
20. The underground garage shall be designed in a manner to accommodate future access to properties to the east of the site, including the provision of knock-out panel(s) and the provision of a public access easement to the satisfaction of the Director of P&Z. The accommodation for the future access shall be depicted and labeled on the final site plan and building permit(s). (P&Z)
21. All stairs shall extend through the roof so that door access to the roof is provided to the satisfaction of the Director of Code Enforcement. (Code)
22. Sheeting and shoring shall not extend beyond the property line; except when the developer has obtained a written release from the adjacent property owners which has been recorded in the land records; or through an approved encroachment process. (Code)
23. The applicant of any building or structure constructed in excess of 10,000 square feet; or any building or structure which constructs an addition in excess of 10,000 square feet shall contact the City of Alexandria Radio Communications Manager prior to submission of final site plan. The proposed project shall be reviewed for compliance with radio requirements of the City of Alexandria to the satisfaction of the City of Alexandria Radio Communications Manager prior to site plan approval. Such buildings and structures shall meet the following conditions:
- a. The building or structure shall be designed to support a frequency range between 806 to 824 MHz and 850 to 869 MHz.
 - b. The building or structure design shall support a minimal signal transmission strength of -95 dBm within 90 percent of each floor area.

- c. The building or structure design shall support a minimal signal reception strength of -95 dBm received from the radio system when transmitted from within 90 percent of each floor area.
- d. The building or structure shall be tested annually for compliance with City radio communication requirements to the satisfaction of the Radio Communications Manager. A report shall be filed annually with the Radio Communications Manager which reports the test findings.

If the building or structure fails to meet the above criteria, the applicant shall install to the satisfaction of the Radio Communications Manager such acceptable amplification systems incorporated into the building design which can aid in meeting the above requirements. Examples of such equipment are either a radiating cable system or an FCC approved type bi-directional amplifier. Final testing and acceptance of amplification systems shall be reviewed and approved by the Radio Communications Manager. (Code)

- 24. In lieu of strict compliance with ladder truck access requirements an alternative compliance proposal is recommended. The proposed massing, articulation of the facade and placement on the lot would be acceptable if the following fire protection and fire fighting features were built into the buildings:
 - a. Enclose all elevator lobbies in smoke tight construction.
 - b. Stair capacity to be designed without taking the sprinklered building exception.
 - c. Fire phones installed on every level in the elevator lobby and the stairs.
 - d. Knock out panels that have an area of 20 feet for every 50 lineal feet of building perimeter.
 - e. Full automatic sprinkler system designed to NFPA 13. (Code)

E. PARKING:

- 25. The applicant shall provide a parking management plan which outlines mechanisms to maximize the use of the underground parking garage, ensure efficient use of the underground parking by the employees, office and retail, patrons to the satisfaction of the Directors of P&Z and T&ES, which shall at a minimum include the following:
 - a. Parking rates for the underground parking shall be market rate, as based on comparable buildings within the City of Alexandria, except that free parking may be provided for short-term visitor parking. All office employees shall be required to pay market rates for parking; no parking may be provided free or at reduced rates or with costs reimbursed by the employer, unless the employer provides an equivalent benefit to all employees who utilize transit options to commute; i.e., if an employer provides a \$100 parking space to an employee free of cost, that employer must also offer a pretax benefit for transit of \$100 to all transit users.
 - b. A minimum of 5% of parking spaces for carpool and vanpool vehicles shall be

- conveniently located adjacent to garage entrances and exits, and/or elevator locations.
- c. The applicant shall provide controlled access into the parking structure.
 - d. The controlled access to the parking structure for short-term spaces shall be designed to allow convenient access for employees, visitors-short-term parking and retail patrons.
 - e. Free parking for retail patrons shall be provided.
 - f. The retail parking spaces within the lower level parking garage shall be reserved for retail patrons and shall include all applicable signage.
 - g. The applicant shall be responsible for all appropriate signage directing customers and office workers to the garage. Freestanding and other signage other than traffic signs shall be prohibited.
 - h. The walls and ceilings in the garages are to be painted white unless the photometric lighting plan demonstrates that sufficient lighting is being provided, to the satisfaction of the Police Chief and the Director of T&ES. (P&Z) (T&ES) (Police)
26. The applicant shall provide off-street parking for all construction workers without charge. For the construction workers who use Metro, DASH, or another form of mass transit to the site, the applicant shall subsidize a minimum of 50% of the fees for mass transit. Compliance with this condition shall be based on a plan, which shall be submitted to the Department of P&Z and T&ES prior to the issuance of the Excavation/Sheeting, and Shoring Permit. This plan shall set forth the location of the parking to be provided at various stages of construction, how many spaces will be provided, how many construction workers will be assigned to the work site, and mechanisms which will be used to encourage the use of mass transit. The plan shall also provide for the location on the construction site at which information will be posted regarding Metro schedules and routes, bus schedules and routes. If the plan is found to be violated during the course of construction, a correction notice will be issued to the developer. If the violation is not corrected within ten (10) days, a "stop work order" will be issued, with construction halted until the violation has been corrected. (P&Z)

F. TRANSIT INCENTIVES

27. The applicant shall create a program and implement incentives to encourage the use of mass transit, carpooling, teleworking, and ridesharing and discouraging the use of single occupancy vehicles to the satisfaction of the Directors of P&Z and T&ES by providing the following:
- a. The applicant shall fund a transportation management account at an annual rate of \$0.11 per occupied square foot of commercial space, which shall increase annually in an amount equal to the CPI Index to be used exclusively for the transportation activities listed below. As determined by the Directors of T&ES

and P&Z, any unencumbered funds remaining in the TMP account at the end of each reporting year may be either reprogrammed for the site's TMP activities during the ensuing year or paid to the City for use in TMP support activities which benefit the site.

- b. Discounted bus and rail fare media shall be sold on-site to employees of the project including during hours that are convenient for residents who work. The fare media to be sold will include, at a minimum, fare media for Metrorail, Metrobus, DASH and any other public transportation system fare media requested by employees and/or the Office of Transit Services and Programs. The availability of this fare media will be prominently advertised.
- c. The project shall have a goal of a minimum of 25% of the employees using transportation other than single-occupancy vehicles during the peak time periods.
- d. A carshare program shall be established and marketed as part of the ridesharing and transit marketing efforts for all buildings. At a minimum at least two parking spaces should be reserved for the location of carshare vehicles. These spaces should be in a convenient location for tenants and residents and the TMP Coordinator will arrange with any of the carshare companies for placement of vehicles in this project. (Currently, Zipcar and Flexcar both have vehicles in the Alexandria area.). For those individuals that take transit, carpool/vanpool, walk, or bike to work the TMP program will pay the registration and annual membership fees (not the usage fees) to use the carshare vehicles.
- e. The applicant shall provide fifteen (15) bicycle parking space(s) within the underground garage and six (6) visitor bicycle parking space(s) on the surface to the satisfaction of the Director of T&ES. The at grade parking bike rack shall be a matte black finish and integrated into the sidewalk and streetscape.
- f. The applicant shall provide three (3) showers per gender to the satisfaction of the Director of T&ES. A minimum of 18 clothes storage lockers per gender shall be installed for every required employee bicycle parking space. The lockers shall be accessible to all tenants of the building to the satisfaction of the Director of T&ES.
- g. The applicant shall participate in Ozone Action Days and other regionally sponsored clean air, transit, and traffic mitigation promotions by advertising such promotions in a manner and at such locations within the building acceptable to the applicant.
- h. The applicant shall participate and cooperate with other developments in the King Street/Carlyle area in a mutually agreed upon cooperative planning and implementation of transportation activities.
- i. The applicant shall implement transit, ride sharing, staggered work hours/compressed work weeks and parking restrictions and shall be promoted to prospective tenants, employers and employees, including the registration of staggered work hour participants, issuing stickers and/or electronic cards to verify vehicles participating in the program and monitoring the program.

- j. All ridesharing program shall be established that includes not only participation in the regional Metropolitan Washington Council of Governments Commuter Connections Program, but also site-specific matching efforts.
- k. A Guaranteed Ride Home Program shall be established and promoted as part of the ridesharing and transit marketing efforts.
- l. At full occupancy of each of each building, a survey of employees shall be conducted to determine the number of employees, their place of residence/employment, modes of transportation, arrival and departure times, willingness and ability to use carpooling and public transit, and such additional information as the City may require. This survey will be conducted annually.
- m. Information about all transit, ridesharing, and other TMP elements shall be distributed and displayed to employers, and employees—including transit schedules, rideshare applications and information, incentive information, parking information, etc. This information shall be kept current. Displays of these brochures and applications shall be provided in a prominent location within each building and a web site with this information and appropriate links to transit providers will be provided and maintained.
- n. A TMP coordinator with experience in this occupation shall be designated for the project upon application for the certificate of occupancy permit for the first building. The TMP coordinator shall have an on-site office, and the name, location and telephone number of the coordinator will be provided to the City at that time, and the City will be notified at the time of any changes. This person will be responsible for implementing and managing all aspects of the TMP and the parking management program for the project, and for providing biannual reports to the Office of Transit and Programs in the Department of T&ES. The biannual reports will include an assessment of the effects of the previous six month's TMP activities on carpooling, vanpooling, and transit ridership; an accounting of receipts and disbursements for any TMP accounts, and a work program for the subsequent six months. (P&Z) (T&ES)

G. VACATION

- 28. The applicant shall pay the fair market value for the vacated right-of-way, as determined by the Director of Real Estate Assessments. (T&ES) (P&Z)
- 29. The applicant shall show all public and private easements on the plat of consolidation. (T&ES)
- 30. The vacated right-of-way shall be consolidated with the adjoining lot, and the plat of consolidation shall be approved by the Departments of P&Z and T&ES prior to release of the final site plan. The approved plat shall be recorded in the Land Records of the City of Alexandria. (T&ES) (P&Z)

31. The applicant shall be responsible for perpetual ownership, development and maintenance of the improvements constructed in the vacated right of way. (T&ES) (P&Z)
32. The property owners shall not use the vacated land area to derive any increased above grade development rights for the lands adjacent to the vacated area, including increased floor area, subdivision rights or additional dwelling units; however, the property owner may develop the below grade area and build improvements. The above grade restrictions and below grade improvement rights shall appear as part of the deed of vacation and shall also appear as a note on the consolidation plat, both of which shall be approved by the Directors of P&Z and T&ES. (T&ES) (P&Z)
33. The City shall own and maintain the sidewalk constructed in the public right-of-way; however, the property owner shall maintain the side walk constructed in the vacated right of way. The sidewalk constructed in the Owner's property shall be privately owned and maintained, and public accessible, which shall be a public access easement that allows the public to use the sidewalk. (T&ES) (P&Z)

H. STREETS

34. No overhangs (decks, bays, columns, post or other obstructions) shall protrude into public easements, pedestrian or vehicular travelways. (T&ES)
35. All driveway entrances, sidewalks, curbing, etc. in public ROW or abutting public ROW shall meet City design standards. (T&ES)
36. A minimum of 30 feet separation between beginning of street corner radius and any driveway apron radius shall be maintained. Additional curb cuts at this location are not recommended as they impede traffic flow. (T&ES)
37. The applicant shall replace existing curb and gutter, sidewalks, and handicap ramps that are in disrepair or broken. (T&ES)
38. Prior to the release of the final site plan, the applicant shall provide a Traffic Control Plan for construction detailing proposed controls to traffic movement, lane closures, construction entrances, haul routes, and storage and staging. (T&ES)
39. All Traffic Control Device design plans, Work Zone Traffic Control plans, and Traffic Studies shall be signed and sealed by a professional engineer, registered in the Commonwealth of Virginia. (T&ES)

40. Show turning movements of standard vehicles in the parking structure. Turning movements shall meet AASHTO vehicular guidelines and shall be to the satisfaction of the Director of T&ES. (T&ES)
41. The slope on the parking ramp to the garage entrance shall not exceed 10 percent. (T&ES)
42. Construction of the development should be restricted to the development and not be allowed to encroach into the public right-of-way. (T&ES)
43. The 5 on-street parking spaces that are proposed to be on the east side of Reineker's Lane shall be opened to the public and not restricted to use by the development. (T&ES)

I. AFFORDABLE HOUSING

44. The developer shall make a voluntary contribution of \$1.50 per gross square foot on the 127,908 gross square feet of permitted ("by right") development, for a total voluntary contribution of \$191,862, consistent with the conclusions of the "Developer Housing Contribution Work Group Report" dated May 2005 and accepted by the Alexandria City Council on June 14, 2005. (Housing)

J. SITE PLAN

45. As part of the request for a certificate of occupancy permit, the applicant shall submit a building, garage, and site location survey to the Department of P&Z for all site improvements. The applicant shall also submit a certification of height for the building as part of the certificate of occupancy for each building(s). The certification shall be prepared and sealed by a registered architect or surveyor and shall state that the height of the building complies with the height permitted pursuant to the approved development special use permit and that the height was calculated based on all applicable provisions of the Zoning Ordinance. (P&Z)
46. The applicant shall submit a wall check to the Department of P&Z prior to the commencement of framing for the building(s). The building footprint depicted on the wall check shall comply with the approved final site plan. The wall check shall also provide the top-of-slab and first floor elevation as part of the wall check. The wall check shall be prepared and sealed by a registered engineer or surveyor, and shall be approved by the City prior to commencement of framing. (P&Z)
47. Depict and label all utilities and the direction of service openings on above grade utilities such as transformers, telephone, HVAC units, and cable boxes. Specifically indicate perimeter clearance/safety zones on plan drawings for utilities requiring

perimeter safety zones, such as transformers. All utilities including but not limited to transformers, telephone and cable boxes shall be screened and shall not be visible from the adjoining streets and shall not conflict with the northern pedestrian connection. As part of the final site plan, the applicant shall coordinate with all applicable utility companies the amount, type, and location of all utilities on the final site plan. If the utilities cannot be located as outlined above, the utilities shall be located underground in vaults which meet Virginia Power standards. (P&Z) (RP&CA)

48. In locations where underground garages are proposed, indicate the location and design of ventilation shafts, egress stairs, and dumpster/service areas. (RP&CA)
49. Provide a lighting plan with the final site plan to verify that lighting meets City standards. The plan shall be to the satisfaction of the Directors of T&ES & P&Z, in consultation with the Chief of Police and shall include the following:
 - a. Clearly show location of all existing and proposed street lights and site lights, shading back less relevant information;
 - b. A lighting schedule that identifies each type and number of fixtures, mounting height, and strength of fixture in Lumens or Watts;
 - c. Manufacturer's specifications and details for all proposed fixtures; and
 - d. A photometric plan with lighting calculations that include all existing and proposed light fixtures, including any existing street lights located on the opposite side(s) of all adjacent streets. Photometric calculations must extend from proposed building face(s) to property line and from property line to the opposite side(s) of all adjacent streets and/or 20 feet beyond the property line on all adjacent properties. Show existing and proposed street lights and site lights. Indicate the type of fixture, and show mounting height, and strength of fixture in lumens or watts. Provide manufacturer's specifications for and installation schedule indicating the number of each fixture to be installed. Provide lighting calculations and photometric plan to verify that lighting meets City Standards. Lighting plan should cover site, adjacent right-of-way and properties.
 - e. Lighting fixtures shall be setback two feet from back of curb. Provide detailed information indicating proposed light pole and foundation in relationship to adjacent grade or pavement. Street light foundations shall be concealed from view.
 - f. Detail information indicating proposed light pole and footing in relationship to adjacent grade or pavement. Pole mounted light foundations shall be concealed from view. (P&Z) (T&ES) (RP&CA) (Police)
50. The applicant shall prepare and submit a plan that delineates a detailed construction management plan for the entire project for review and approval by the Directors of P&Z, T&ES, and Code Enforcement prior to the release the final site plan. Before

commencing any clearing or grading of the site, the applicant shall hold a meeting with notice to all adjoining property owners to review the location of construction worker parking, plan for temporary pedestrian and vehicular circulation, and hours and overall schedule for construction. The Departments of P&Z and T&ES shall be notified of the date of the meeting before the permit is issued. Copies of plans showing the hauling route, construction worker parking, and temporary pedestrian and vehicular circulation shall be posted in the construction trailer and given to each subcontractor before they commence work. (P&Z) (T&ES)

51. The applicant shall identify a person who will serve as a liaison to the community throughout the duration of construction. The applicant shall develop a plan of communication with the community in consultation with the Departments of P&Z and T&ES. The name and telephone number of this individual shall be provided in writing to residents, property managers, and business owners whose property abuts the site, and to the Directors of P&Z and T&ES. (P&Z)(T&ES)
52. A temporary informational sign shall be installed on the site prior to the approval of the final site plan for the project and shall be displayed until construction is complete or replaced with a marketing sign incorporating the required information; the sign shall notify the public of the nature of the upcoming project and shall provide a phone number for public questions regarding the project. (P&Z)(T&ES)
53. The subdivision plats, easements and/or dedication shall be submitted as part of the submission for first final site plan and shall be approved and recorded prior to the release of the final site plan. (P&Z)
54. All proposed dumpsters and recycling facilities shall be shown on the final site plan. Such facilities shall be located in locations not visible from public rights-of-ways to the extent possible, and shall be screened to the satisfaction of the Directors of P&Z and T&ES. (P&Z) (T&ES)
55. The applicant shall be allowed to make minor adjustments to the building location if the changes do not result in off-street parking or open space below that required by the Zoning Ordinance and do not result in an increase in building height or floor area ratio. (P&Z)
56. The site is located on marine clay areas as delineated on the City map of marine clay areas. Provide a geotechnical report, including recommendations from a geotechnical professional for proposed cut slopes and embankments. Construction methodology and erosion and sediment control measures must account for the presence of marine clay or highly erodible soils. (T&ES)

57. No major construction staging will be allowed from Duke Street. Applicant to meet with T&ES to discuss construction staging activities prior to release of any permits for ground disturbing activities. (T&ES)
58. Applicant shall connect to the City's sanitary sewer system via the existing minimum 10" sewer line along either Duke Street or Reinekers Lane. In the event that the existing sewer line is smaller than 10" in diameter, the applicant shall replace the line with a minimum 10" diameter line. (T&ES)
59. All private utilities are to be located outside of the public right of way and public utility easements. (T&ES)
60. Show all existing and proposed, both public and private. (P&Z) (T&ES)
61. Applicant shall underground all the utilities serving the property. (T&ES)
62. The applicant shall provide an estimate of the average day and peak hour wastewater flow discharged upstream of the development site under existing conditions and the current contribution of sanitary flow from the development site to the Alexandria Sanitation Authority (ASA) Trunk Sewer to the satisfaction of the Director of T&ES using the factors described below:
 - a. The sewer connection shall be designed for the ultimate build-out conditions.
 - b. Recommended average design flows:
 - i. Residence general 100 gpcd
 - ii. Single Family Home 350 gpd/unit
 - iii. Townhouse 350 gpd/unit
 - iv. Garden Apartment 300 gpd/unit
 - v. High Rise Residential 300 gpd/unit
 - vi. Office / Commercial 200 gpd/1000 sq. ft. (T&ES)
63. For any other type of development not covered above, the applicant may obtain contributing sanitary flow from the Commonwealth of Virginia, State Water Control Board, Sewage Collection and Treatment (SCAT) Regulations or propose the criteria to be used for estimation of sanitary flows to the satisfaction of the Director of T&ES.
 - a. The sanitary sewers shall be designed for maximum hour flow.
 - b. A peak factor of 4.0 shall be used for laterals and sub-mains. A peak factor of 3.0 may be used for mains. (T&ES)

64. The applicant shall estimate additional average day and peak hour wastewater flow to be discharged into the trunk sewer from the proposed development site under proposed conditions to the satisfaction of the Director of T&ES using the factors described above. The applicant shall provide adequate sanitary outfall analysis sufficient to determine existing and future flows in the sewers to be used by the applicant that are tributary to the City of Alexandria's sanitary interceptor sewer system. (T&ES)

65. The applicant shall provide the following sanitary sewer improvements and information to the satisfaction of the Director of T&ES. The following information shall be depicted on the final site plan and addressed to the satisfaction of the Director of T&ES prior to the release of the final site plan and/or building permits:
 - a. The applicant shall provide the current (measured, if possible) and post development estimates of average day and peak sanitary flow. The increased peak flow will be placed in the City of Alexandria wastewater flow capacity registry to determine that the City has sufficient treatment capacity available in the Alexandria Sanitation Authority (ASA) Advanced Wastewater Treatment Plant (AWWTP). The City of Alexandria's Allocated Capacity is 21.6 MGD
 - b. Sanitary sewer plumbing fixtures and drains located below first floor (including parking structures) shall have in-structure or on-site pumped discharge to the City's gravity collection system. The pumped facilities shall be provided with a standby source of power (i.e., battery or generator).
 - c. The property Owner shall be responsible for the perpetual ownership, capital, and maintenance and Operation of the pumps and appurtenances.
 - d. The applicant shall perform the sanitary sewer adequate outfall analysis to prove that sufficient transport capacity exists in the sanitary trunk sewer serving the site.
 - e. No foundation drain shall be connected to City or ASA sanitary sewer. (T&ES)

66. The applicant shall show sanitary and storm sewer in plan and profile in the first final submission and cross reference the sheets on which the plan and profile is shown, if plan and profile is not shown on the same sheet. Clearly label the sanitary and storm sewer plan and profiles. Provide existing and proposed grade elevations along with the rim and invert elevations in the first final submission of all existing and proposed sanitary and storm sewer piping on the respective profiles. Use distinctive stationing for various sanitary and storm sewers in plan and use the corresponding stationing in respective profiles. (T&ES)

67. The applicant shall show water line in plan and profile in the first final submission and cross reference the sheets on which the plan and profile is shown, if plan and profile is not shown on the same sheet. Clearly label the water line plan and profile. Provide existing and proposed grade elevations along with the invert elevations in the first final

submission of all existing and proposed water line piping on profiles. Provide rim elevations of gate wells, where applicable. Use distinctive stationing for various water lines in plan and use the corresponding stationing in respective profiles. (T&ES)

68. The solid waste and recycling facilities shall be privately maintained. (T&ES)
69. There are existing 24" and 12" water mains on Holland Lane that tie into the existing 20" main on Duke Street. The mains on Holland Lane will need to be crossed when the proposed 10" force main is installed. Please show these existing 24" and 12" mains on the plans to help prevent damage to them. Show test pits at crossings. (VAWC)
70. The existing water mains at the intersection of Reineker's Lane and Prince Street are not shown correctly. VAWC will forward an as built drawing to Bowman Consulting. (VAWC)
71. Provide tap to the existing 8" water main on Reineker's Lane for the fire and domestic services to the building, or if this is not possible and the proposed 8" water main needs to be installed, offset it from the existing 6" water main. (VAWC)
72. Hydraulic calculations (computer modeling) will be completed to verify main sizes upon final submittal of the site plan. Profiles will be required for hydraulic calculations. (VAWC)
73. VAWC will require a copy of the Code Enforcement approved needed fire flow calculations for this project. (VAWC)
74. A double detector check backflow prevention device is required on all fire services. If located inside the premise, it shall have a remote reading meter in a separate accessible room. (VAWC)
75. Please add the following note to the site plan and utility plan sheets, "All water facility construction shall conform to Virginia American Water Standards and Specifications." (VAWC)
76. The project site lies within the Braddock Road West (Timber Branch) Watershed thus stormwater quantity controls shall be designed to demonstrate that post development stormwater runoff does not exceed 90 percent of the existing runoff quantities for both the 2-year and 10-year storm events. Develop shall comply with the peak flow requirements of Article XIII of the Alexandria Zoning Ordinance. If combined uncontrolled and controlled stormwater outfall is proposed, the peak flow requirements of Article XIII of the Alexandria Zoning Ordinance shall be met. (T&ES)

77. The storm water collection system is located within the Timber Branch watershed and it appears that this parcel drains into Hooff's Run. All on-site storm water curb inlets and public curb inlets within 50 feet of the property line shall be duly marked using standard City markers, or to the satisfaction of the Director of T&ES. (T&ES)
78. The City of Alexandria's storm water management regulations regarding water quality are two-fold: first, phosphorus removal requirement and second, water quality volume default. Compliance with the phosphorus requirement does not relieve the applicant from the water quality default requirement. The water quality volume determined by the site's proposed impervious area shall be treated in a Best Management Practice (BMP) facility. (T&ES)
79. The storm water Best Management Practices (BMPs) required for this project shall be constructed and installed under the direct supervision of the design professional or his designated representative. Prior to release of the performance bond, the design professional shall submit a written certification to the Director of T&ES that the BMPs are:
 - a. Constructed and installed as designed and in accordance with the approved Final Site Plan.
 - b. Clean and free of debris, soil, and litter by either having been installed or brought into service after the site was stabilized. (T&ES)
80. The Applicant shall submit a storm water quality BMP Maintenance Agreement with the City to be reviewed as part of Final Site Plan #2. It must be executed and recorded with the Land Records Division of Alexandria Circuit Court prior to approval of the final site plan. (T&ES)
81. The Applicant shall be responsible for maintaining storm water Best Management Practices (BMPs) until sale to a private owner(s). Prior to transferring maintenance responsibility for the BMPs to the owner(s), the Applicant shall execute a maintenance service contract with a qualified private contractor for a minimum of three years, and transfer the contract to the owner(s). A copy of the contract shall also be placed in the BMP Operation and Maintenance Manual. Prior to release of the performance bond, a copy of the maintenance contract shall be submitted to the City. (T&ES)
82. The Developer shall furnish the owner(s) with an Owner's Operation and Maintenance Manual for all Best Management Practices (BMPs) on the project. The manual shall include at a minimum: an explanation of the functions and operations of the BMP(s); drawings and diagrams of the BMP(s) and any supporting utilities; catalog cuts on maintenance requirements including mechanical or electrical equipment; manufacturer contact names and phone numbers; a copy of the executed maintenance service contract; and a copy of the maintenance agreement with the City. (T&ES)

83. Prior to release of the performance bond, a copy of the Operation and Maintenance Manual shall be submitted to the Division of Environmental Quality on digital media. (T&ES)
84. Prior to release of the performance bond, the Applicant is required to submit a certification by a qualified professional to the satisfaction of the Director of T&ES that any existing storm water management facilities adjacent to the project and associated conveyance systems were not adversely affected by construction operations and that they are functioning as designed and are unaffected by construction activities. If maintenance of the facility or systems were required in order to make this certification, provide a description of the maintenance measures performed. (T&ES)
85. If fireplaces are utilized in the development, the Applicant is required to install gas fireplaces to reduce air pollution and odors. Animal screens must be installed on chimneys. (T&ES)
86. Contractors shall not cause or permit vehicles to idle for more than 10 minutes when parked. (T&ES)
87. The Applicant shall present a disclosure statement to potential buyers disclosing the following to the satisfaction of the Director of P&Z and the City Attorney:
 - a. That Duke Street is a major four-lane arterial and that future traffic is expected to increase significantly as development along Duke Street continues. (P&Z) (City Attorney)

K. STORMWATER

88. The downspouts and sump pump discharges, if any shall be piped to the storm sewer outfall. (T&ES)
89. The applicant is advised that all storm water designs that require analysis of pressure hydraulic systems and/or inclusion and design of flow control structures must be signed and sealed by a professional engineer, registered in the Commonwealth of Virginia. The design of storm sewer shall include the adequate outfall, inlet, and hydraulic grade line (HGL) analyses subject to the satisfaction of the Director of T&ES. Provide appropriate reference and/or source used to complete these analyses. If applicable, the Director of T&ES may require resubmission of all plans that do not meet this standard. (T&ES)
90. Provide proposed elevations (contours and spot shots) in sufficient details on grading plan to clearly show the drainage patterns. (T&ES)

91. Plan must demonstrate to the satisfaction of the Director of T&ES that a non-erosive adequate storm water outfall is available to the site or the developer is to design and build non-erosive on-site or off-site improvements to discharge to an adequate outfall. (T&ES)
92. The minimum diameter for public storm sewers shall be 18-inches. The storm sewers immediately upstream of the first manhole in the public right of way shall be owned and maintained privately. (T&ES)

L. CONSTRUCTION

93. A “Certified Land Disturber” (CLD) shall be named in a letter to the Division Chief of C&I prior to any land disturbing activities. If the CLD changes during the project, that change must be noted in a letter to the Division Chief. A note to this effect shall be placed on the Phase I Erosion and Sediment Control sheets on the site plan. (T&ES)
94. During the construction phase of this development, the site developer, their contractor, certified land disturber, or owner’s other agent shall implement a waste and refuse control program. This program shall control wastes such as discarded building materials, concrete truck washout, chemicals, litter or trash, trash generated by construction workers or mobile food vendor businesses serving them, and all sanitary waste at the construction site and prevent offsite migration that may cause adverse impacts to neighboring properties or to the environment to the satisfaction of Directors of Transportation and Environmental Services and Code Enforcement. All wastes shall be properly disposed offsite in accordance with all applicable federal, state and local laws. (T&ES)

M. LEGAL/PROCEDURAL:

95. Any inconsistencies between the various drawings submitted by the applicant shall be reconciled to the satisfaction of the Directors of P&Z and T&ES. (P&Z) (T&ES)
96. Temporary construction and/or sales trailer(s) shall be permitted and the period such structures are to remain on the site, size and site design for such structures shall be subject to the approval of the Director of P&Z. The trailer(s) shall be removed prior to the issuance of a certificate of occupancy permit for the building. (P&Z)

DEPARTMENT CODE COMMENTS

Legend: C - code requirement R - recommendation S - suggestion F - finding

Transportation & Environmental Services:

- C-1 Bond for the public improvements must be posted prior to release of the plan.
- C-2 All downspouts must be connected to a storm sewer by continuous underground pipe.
- C-3 The sewer tap fee must be paid prior to release of the plan.
- C-4 All easements and/or dedications must be recorded prior to release of the plan.
- C-5 Plans and profiles of utilities and roads in public easements and/or public right-of-way must be approved prior to release of the plan.
- C-6 All drainage facilities must be designed to the satisfaction of T&ES. Drainage divide maps and computations must be provided for approval.
- C-7 All utilities serving this site to be underground.
- C-8 Provide site lighting plan.
- C-9 Plan shall comply with the Chesapeake Bay Preservation Act in accordance with Article XIII of the City's zoning ordinance for storm water quality control.
- C-10 Provide a phased erosion and sediment control plan consistent with grading and construction plan.
- C-11 Per the Memorandum To Industry, dated July 20, 2005, the applicant is advised regarding a requirement that applicants provide as-built sewer data as part of the final as-built process. Upon consultation with engineering firms, it has been determined that initial site survey work and plans will need to be prepared using Virginia State Plane (North Zone) coordinates based on NAD 83 and NAVD 88. Control points/Benchmarks which were used to establish these coordinates should be referenced on the plans. To insure that this requirement is achieved, the applicant is requested to prepare plans in this format including initial site survey work if necessary. (Site Plans)
- C-12 Ensure all discharges are in accordance with City of Alexandria Code 4035.

- C-13 All drainage facilities must be designed to the satisfaction of T&ES. Drainage divide maps and computations must be provided for approval.
- C-14 The applicant shall comply with the City of Alexandria's Noise Control Code, Title 11, Chapter 5, which sets the maximum permissible noise level as measured at the property line.
- C-15 The applicant must comply with the Article XIII of the City of Alexandria Zoning Ordinance, which includes requirements for storm water pollutant load reduction, treatment of the water quality volume default, and storm water quantity management.
- C-16 The applicant must comply with the City of Alexandria, Erosion and Sediment Control Code, Section 5, Chapter 4. This includes naming a Responsible Land Disturber on the Erosion and Sediment Control sheets prior to engaging in land disturbing activities in accordance with Virginia Erosion and Sediment Control Law.
- C-17 All required permits from Virginia Department of Environmental Quality, Environmental Protection Agency, Army Corps of Engineers, Virginia Marine Resources must be in place for all project construction and mitigation work prior to release of the final site plan. This includes the state requirement for a VSMP permit for land disturbing activities greater than 2500 SF.
- R-1 P&Z to coordinate with T&ES to ensure that planned flow capacity does not exceed City of Alexandria allotted ASA plant capacity of 20.5. Validate that the increased flow into the Holmes Run Trunk Sewer, Potomac Interceptor, and the Commonwealth Interceptor will not exceed City of Alexandria allotted flow capacity.
- R-2 P&Z to coordinate with T&ES to ensure in writing to ASA that additional flow planned does not exceed flow capacity in ASA Interceptors and Trunk Sewers during wet and average flow conditions. Provide written notice from City that flow capacities to ASA Interceptors and Trunk Sewers from proposed development will not exceed City's wet and average allocated flow conditions.
- F-1 Water quality volume is generated by the impervious surface on the site. Applicant's BMP is treating 0.686 acres of the water quality volume being generated. There remains 0.104 acres of impervious surface, from which the water quality volume is not being treated. Applicant is encouraged to carefully explore mechanisms to treat this volume. Should this be impossible applicant is referred to City of Alexandria, Article XIII, Environmental Management

Ordinance, Section 13-110(A), *Alternate stormwater management equivalency options and establishment of the Alexandria Water Quality Improvement Fund*. To employ either option, applicant shall follow the guidance provided in Section 13-110(D) and submit a letter to Claudia Hamblin-Katnik, Watershed Program Administrator, 301 King Street, Room 3900, Alexandria, VA 22314 outlining his intent.

- F-2 The plan proposes to extend the limits of the below grade parking structure beyond the property line of the development along Reineker's Lane at the southeast corner of the intersection of Prince Street at Reineker's Lane.
- F-3 The plan proposes to reduce the curb-to-curb width of a section of Reineker's Lane south of Prince Street from 35.6 feet to about 25 feet.
- F-4 The plan proposes to provide 5 on-street parking spaces within the public right-of-way of a recessed parking bay along the east side of Reineker's Lane.
- F-5 Revise lane configuration at the intersection of Diagonal and Daingerfield Roads. Eastbound Diagonal Road has a left lane, a shared left and thru lane and a right turn lane. Northbound Daingerfield Road has a shared left, thru and right turn lane. Revise results accordingly.
- F-6 Revise existing conditions HCM analysis to reflect existing spits, pedestrian intervals, phasing, and clearance intervals. Signal timings are attached. For all 2010 and 2020 conditions, use pedestrian and clearance intervals from existing conditions. Revise HCM analysis to accommodate pedestrian calls and pedestrian intervals.

Code Enforcement:

The following are repeat comments. Updated comments are in **BOLD**.

- F-1 This structure is not ladder truck accessible. See C-1 below.
- F-2 Will ownership of the parking structure and office buildings be a sole owner or is separate ownership for each structure proposed. Finding resolved, Building and parking garage owned by same entity.
- C-1 Building is over 50 feet in height and as such is required to have ladder truck access to the front and the rear of the buildings by public roads or recorded emergency vehicle easements (eve). For a building face to be considered

accessible by a ladder truck the curb line shall be at least 15 feet and no more than 30 feet from the face of the building. The face of the building may not articulate back into the mass of the building more than 7 feet horizontally in the first 75 feet of vertical dimension of the building. Alternatives that demonstrate equivalency to this requirement will be considered on a case by case basis. The current proposed building articulation and design of this building does not meet the intent of this requirement and therefore is not ladder truck accessible. This issue requires resolution. Condition not met.

- C-2 New construction must comply with the current edition of the Uniform Statewide Building Code (USBC). Condition met.
- C-3 The developer shall provide a separate Fire Service Plan which illustrates: a) emergency ingress/egress routes to the site; b) two fire department connections (FDC) to the building, one on each side/end of the building; c) fire hydrants located within one hundred (100) feet of each FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a twenty-two (22) foot minimum width; f) all Fire Service Plan elements are subject to the approval of the Director of Code Enforcement. Condition met.
- C-4 Prior to submission of the Final Site Plan #1, the developer shall provide a fire flow analysis by a certified licensed fire protection engineer to assure adequate water supply for the structure being considered. Acknowledged by applicant.
- C-5 The final site plans shall show placement of the fire easement signs. Acknowledged by applicant.
- C-6 Prior to the issuance of a demolition permit or land disturbance permit, a rodent abatement plan shall be submitted to Code Enforcement that will outline the steps that will be taken to prevent the spread of rodents from the construction site to the surrounding community and sewers. Acknowledged by applicant.
- C-7 Roof drainage systems must be installed so as neither to impact upon, nor cause erosion/damage to adjacent property. Acknowledged by applicant.
- C-8 These structures contain mixed use groups and are subject to the mixed use and occupancy requirements of USBC 302.3. Acknowledged by applicant.

- C-9 Required exits, parking, and accessibility within the building for persons with disabilities must comply with USBC Chapter 11. Handicapped accessible bathrooms shall also be provided. Condition met.
- C-10 The public parking garage floor must comply with USBC 406.2.6 and drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers as provided for in the plumbing code (USBC 2901). This parking garage is classified as an S-2, Group 2, public garage. Condition not met. Applicant shall show correct sheets. The sheets listed A2.01 - A2.04 do not show parking garage. Condition met.
- C-11 Enclosed parking garages must be ventilated in accordance with USBC 406.4.2. Condition not met, Architectural Cover Sheet not provided. Condition met.
- C-12 This garage is required to have an automatic sprinkler system throughout the structure in compliance with USBC 406.4.1 and 903.2.11. Acknowledged by applicant.
- C-13 The proposed building must comply with the requirements of HIGH-RISE buildings (USBC 403). Acknowledged by applicant.
- C-14 A soils report must be submitted with the building permit application. Acknowledged by applicant.
- C-15 Provide Stairway Identification. A sign shall be provided at each floor landing in interior vertical exit enclosures connecting more than three stories designating the floor level, the terminus of the top and bottom of the stair enclosure and the identification of the stair. The signage shall also state the story of, and the direction to the exit discharge and the availability of roof access from the stairway for the fire Department, in accordance with USBC 1019.1.7.

Health Department:

- C-1 An Alexandria Health Department Permit is required for all regulated facilities.
- C-2 Permits are non-transferable.
- C-3 Permits must be obtained prior to operation.
- C-4 Five sets of plans are to be submitted to and approved by this department prior to construction of any facility.

- C-5 Plans for food facilities must comply with the Alexandria City Code, Title 11, Chapter 2, Food and Food Establishments. There is a \$135.00 fee for review of plans for food facilities.
- C-6 Pool plans must comply with Title 11, Chapter 11, Swimming Pools. Tourist establishment pools must have six (6) sets of plans submitted.
- C-7 Personal grooming facilities must comply with Title 11, Chapter 7, Personal Grooming Establishments.
- C-8 Tanning Salons must meet State Code Title 59.1, Chapter 24.1, Tanning Facilities.
- C-9 Massage facility plans must comply with Title 11, Chapter 4.2, Massage Regulations. All massage therapists must possess a current massage therapist certification, issued by the Commonwealth of Virginia in accordance with the Code of Virginia Chapter 599, Sec. 54.1-3029 and must possess an Alexandria Massage permit in accordance with Alexandria City Code Title 11, Chapter 4.2 prior to engaging in any massage activity.
- C-10 Coin-operated dry cleaning facility plans must comply with Title 9, Chapter 4, Coin Operated Dry Cleaning Establishments.
- C-11 Coin-operated laundry plans must comply with Title 9, Chapter 5, Coin Operated Laundries.
- C-12 Hotels/Motels must comply with State Code 35.1, Hotels, Restaurants, Summer Camps, and Campgrounds.
- C-13 Provide a menu or list of foods to be handled at this facility to the Health Department prior to opening.
- C-14 Food must be protected to the point of service at any outdoor dining facility.

Police Department:

- R-1 A security survey is to be completed for construction trailers that are placed on the site. This is to be completed as soon as the trailers are placed on site by calling the Community Relations Unit at 703- 838-4520.

- R-2 The applicant of the spa is to contact the Community Relations Unit of the Alexandria Police Department at 703-838-4520 regarding a security survey for the business.
- R-3 Trees should not be planted under or near light poles. Trees planted under or near light poles counteract the effectiveness of light illumination when they reach full maturity. The light globes will only illuminate the tree canopy. This counteracts the effectiveness of light illumination nullifying the light's purpose.
- R-4 Maintain tree canopies at least 6-feet above grade level as they mature to allow for natural surveillance.
- R-5 Shrubs higher than 3 feet should not be planted within 6 feet of walkways. Shrubs higher than 3 feet provide cover and concealment for potential offenders.
- R-6 The proposed shrubbery should have a natural growth height of no more than 2 ½ to 3 feet with a maximum height of 36 inches when it matures and should not hinder the unobstructed view of patrolling law enforcement vehicles.
- R-7 For the safety of the persons using the proposed garage, it is recommended that the lighting for the parking garage be a minimum of 5.0 foot candle minimum maintained.
- R-8 For the safety of the persons using the proposed garage, it is recommended that the builder construct stairwells to be visible, without solid walls. This can be accomplished whether the stairwells are located inside the parking garage or in an open-above ground garage. This parking garage was designed with CPTED strategies in mind. This stairwell is designed to be open, well lit and very visible. One can see into and out of the stairwell as well as having the ability to see from one landing to the next from within the stairwell. It is used as a design feature by its location and layout. This natural surveillance aides in the ability of a person to detect possible criminal acts.
- R-9 It is recommended that the applicant meet the lighting standard set by Transportation & Environmental Services. The Alexandria Lighting Standard set by T&ES for "office" parking areas is 1.0 footcandle minimum maintained. The lighting should be consistently uniform throughout the parking lot and on the periphery.
- R-10 In reference to comment #13 under SUP #2007-0018 - if a restaurant leases retail space and decides to sell alcohol, then the restaurant applicant should apply for "ABC" consideration separate and apart from this SUP application.

- R-11 In reference to Encroachment #2007-0003, the Police Department has no comments or objections to the encroachment.

Historic Alexandria (Archaeology):

- F-1 This property was the site of the Bruin Slave Jail in the early 19th century and has high potential to yield significant archaeological resources.
- F-2 The applicant has hired Louis Berger and Associates to conduct the archaeological work on this project. The first phase of the archaeological investigation has been completed, but additional fieldwork and documentary research is required.
- C-1 To insure that significant information is not lost as a result of the current development project, the applicant must hire an archaeological consultant to complete a Documentary Study and an Archaeological Evaluation. Contact Alexandria Archaeology to obtain a scope of work for this investigation. If significant resources are discovered, the consultant must complete a Resource Management Plan, as outlined in the *City of Alexandria Archaeological Standards*. Preservation measures presented in the Resource Management Plan, as approved by the City Archaeologist, will be implemented.
- C-2 All archaeological preservation measures must be completed prior to ground-disturbing activities (such as coring, grading, filling, vegetation removal, undergrounding utilities, pile driving, landscaping and other excavations as defined in Section 2-151 of the Zoning Ordinance). To confirm, call Alexandria Archaeology at (703) 838-4399.
- C-3 Call Alexandria Archaeology immediately (703-838-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds.
- C-4 The statements in C-2 and C-3 above must appear in the General Notes of all site plans and on all site plan sheets that involve demolition or ground disturbance (including sheeting and shoring and grading) so that on-site contractors are aware of the requirements. Additional statements to be included on the Final Site Plan will be determined in consultation with Alexandria Archaeology.
- C-5 Certificates of Occupancy will not be issued for this property until the final archaeological report has been received and approved by the City Archaeologist.

- C-6 If warranted by the City Archaeologist, the developer will erect a historic marker on the property according to specifications provided by Alexandria Archaeology. The marker will highlight the historical and archaeological significance of the property.
- C-7 If warranted by the City Archaeologist, the developer will produce a booklet for the public on the history and archaeology of the property, according to specifications provided by Alexandria Archaeology.
- R-1 All archaeological work will be carried out in accordance with the *City of Alexandria* Archaeological Standards and is subject to the approval of the City Archaeologist. The applicant should not allow any other metal detection to be conducted on the property, unless authorized by Alexandria Archaeology.

Parks & Recreation

- C-1 Provide the following notes on drawings:
- i) "Specifications for plantings shall be in accordance with the current and most up to date edition of ANSI-Z60.1, The American Standard for Nursery Stock as produced by the American Association of Nurserymen; Washington, DC."
 - ii) "In lieu of more strenuous specifications, all landscape related work shall be installed and maintained in accordance with the current and most up-to-date edition (at time of construction) of Landscape Specification Guidelines as produced by the Landscape Contractors Association of Maryland, District of Columbia and Virginia; Gaithersburg, Maryland."
 - iii) "Prior to commencement of landscape installation/planting operations, a pre-installation/construction meeting will be scheduled and held with the City's Arborist and Landscape Architects to review plant installation procedures and processes."
 - iv) "A certification letter for tree wells, tree trenches and plantings above structure shall be provided by the project's Landscape Architect. The letter shall certify that all below grade construction is in compliance with approved drawings and specifications. The letter shall be submitted to the City Arborist and approved prior to approval of the last and final Certificate of Occupancy for the project. The letter shall be submitted by the owner/applicant/successor and sealed and dated by the project's landscape architect."
 - v) "As-built drawings for this landscape and/or irrigation/water management system will be provided in compliance with City of Alexandria Landscape Guidelines. As-built drawings shall include clear identification of all variation(s) and changes from approved drawings including location, quantity and specification of all project elements."

ATTACHMENT #1

